

# Conservation Management Plan for the Site of Lung Tsun Stone Bridge



November 2009

## Executive Summary

1. The Lung Tsun Stone Bridge was a landing pier built between 1873 and 1875 at the coastline of the old Kowloon City. The Stone Bridge was originally about 210 metres in length and from 2.6 to 4 metres in width. It was buried due to Kai Tack Bunds in the 1920s and the former airport development during the Japanese Occupation.
2. In April 2008, the remnants of the Bridge were first identified during an archaeological investigation under the Environmental Impact Assessment (EIA) study for the Kai Tak Development (KTD). Further investigation to ascertain the extent of Bridge remnants and *in-situ* preservation of the Bridge remnants were recommended in the approved EIA report of KTD.
3. Apart from the extant sections of the Stone Bridge, the remnants of the Pavilion for Greeting Officials, Former Kowloon City Pier and segments of 1924 and 1930s seawalls were also identified by the further investigation. Levels of significance are assigned to the archaeological features discovered in this document. The extant sections of the Stone Bridge together with the remnants of the Pavilion for Greeting Officials are of high significance. The remnants of the Former Kowloon City Pier and seawalls segments of 1924 and 1930s are classified as medium and low significance respectively.
4. A set of conservation guidelines, based on recognized charters and principles, is proposed as a framework to guide the conservation, management, maintenance and interpretation of the remnants in future. It is proposed that the remnants of original Lung Tsun Stone Bridge should be preserved *in-situ*. Moreover, no speculative recreation or excessive intervention would be allowed at the area buried with the original Bridge's remnants.

## 摘要

1. 龍津石橋始建於 1873 至 1875 年，是位於舊九龍城岸邊的登岸碼頭。該石橋原長約 210 米，闊約 2.6 至 4 米。其後，石橋因 20 世紀 20 年代啟德濱填海工程及日據時期機場擴建而被埋。
2. 2008 年 4 月，因應啟德發展計劃環境影響評估所進行的考古調查，首次發現龍津石橋的遺跡。獲批准的啟德發展計劃環境影響評估報告建議龍津石橋作原址保存，並進行更深入的考古調查，以確定石橋遺跡的範圍。
3. 除了龍津石橋的遺跡外，進一步的考古調查還發現其他遺跡，包括接官亭、前九龍城碼頭、1924 和 1930 年代的海堤。本文件就遺跡的重要性分為 3 個等級。龍津石橋及接官亭遺跡的重要性屬高等級；前九龍城碼頭、1924 和 1930 年代的海堤其重要性則分別屬中及低等級。
4. 依據公認的文物保護憲章及準則，本文件提出一些保護指引，以作這些遺跡日後的保護、管理、修繕及詮釋方針。指引提出原龍津石橋遺跡須作原址保存。此外，石橋遺跡被埋的位置，不應推測性重建或存有過份干預的措置。

## CONSERVATION MANAGEMENT PLAN FOR THE SITE OF LUNG TSUN STONE BRIDGE

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## **1. INTRODUCTION**

### **1.1 Reasons for the Conservation Management Plan**

The original site of Lung Tsun Stone Bridge or called *Longjin* Bridge (the Site) is an important historic site with high heritage value in Hong Kong. The Site had been modified substantially over years from a landing pier to a reclaimed area forming part of Kai Tak Airport. In 1998, the Kai Tak Airport was relocated to Chek Lap Kok Island, thus offering an opportunity for new development in the Kai Tak Area. In April 2008, the remnants of Lung Tsun Stone Bridge were first identified through the archaeological impact assessment (AIA) of the Kai Tak Development (KTD). Hence, the Environmental Impact Assessment (EIA) report for KTD recommended preserving the Lung Tsun Stone Bridge *in-situ* as part of the KTD after further investigation to demarcate the extent of Bridge remains.

In order to conserve the extant remains of Lung Tsun Stone Bridge uncovered by archaeological investigations, the Antiquities and Monuments Office of Leisure and Cultural Services Department (AMO, LCSD) prepared this Conservation Management Plan (CMP) so that relevant government departments/agencies and authorities, stakeholders as well as the public can have a better understanding about the site, its significance and possible issues involved with the future KTD.

### **1.2 Scope of the Study**

This CMP covers the archaeological findings of Lung Tsun Stone Bridge, Lung Tsun Pavilion/Pavilion for Greeting Officials and Former Kowloon City Pier identified by the archaeological investigations conducted from April 2008 to February 2009. The assessment of history and context will also include information about the Kowloon Walled City and Kai Tak Airport so that an understanding of the context can be attained. As there is a large amount of background research and study that has already been carried out by various scholars and institutes, this document will summarize and make reference to the existing information so as to avoid unnecessary duplication.

### **1.3 Methodology**

This CMP is a brief appraisal of the history and development of the Site with an emphasis on assessing and understanding the significance of the Site. The assessment of the site is based on the archaeological findings yielded by field investigations, basic documentary research and a brief analysis of the surrounding's history. This assessment of significance is then used to identify any vulnerabilities within the Site as well as any possible future opportunities. Therefore, this information is used for creating a set of guidelines intended to inform the future use, maintenance and possible development of the Site.

This CMP comprises the following sections and reference materials:-

#### **Introduction**

A summary of basic information, scope of the study, methodology and relevant documents.

#### **Understanding the Site**

A description of the Site, including location, use and management and gaps in knowledge. Also included is a descriptive summary of the history and development of the Site, including key historic events. The historic outline is presented chronologically.

#### **Assessment of Significance**

Sets out the significance of the Site based on values such as history, archaeology, architecture or culture.

#### **Vulnerabilities and Issues**

Describes various attributes of the Site as well as addressing issues of the existing the KTD planned infrastructure and land use.

#### **Opportunities**

Discusses the opportunities available on the Site, especially with regards to the future development in the Kai Tak Area.

#### **Conservation Guidelines for the Site**

Sets out the guidelines to protect and manage the Site.

## **Conclusion**

A closing statement which assesses the information included within this document.

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## **1.4 Relevant Documents**

The relevant EIA reports, feasibility studies and archaeological investigations for the former Kai Tak Airport are listed as follows:-

- Ove Arup & Partners Hong Kong Ltd. 2001. *Comprehensive Feasibility Study for the Revised Scheme of Southeast Kowloon Development - Cultural Heritage Impact Assessment.*
- Archaeological Assessments Ltd. 2002. *Comprehensive Feasibility Study for the Revised Scheme of Southeast Kowloon Development - Archaeological investigation.*
- Environmental Resources Management HK Ltd. 2003. *Southeast Kowloon Development, Site Investigation at North Apron of Kai Tak Airport: Archaeological Investigation - Findings for Trenches AT1-AT10.*
- Archaeological Assessments Ltd. 2008. *Archaeological Impact Assessment Report for Kai Tak Development Engineering Study cum Design and Construction of Advance Works "Investigation, Design and Construction.*
- Maunsell Consultants Asia Limited. 2009. *Further Archaeological Excavation Report for Kai Tak Development Engineering Study cum Design and Construction of Advance Works - Investigation, Design and Construction.*

## 2. UNDERSTANDING THE SITE

### 2.1 Location and Brief Description

2.1.1 The Lung Tsun Stone Bridge (龍津石橋) was a landing pier, linking the main gate (East Gate) of the Kowloon Walled City (九龍寨城 or 九龍城) and the Kowloon Street (九龍街) to the coast in late 19<sup>th</sup> Century. According to the text records of two stone tablets about the origin of Lung Tsun Stone Bridge, this stone bridge was first constructed between 1873 and 1875. It was originally about 210 metres in length and from 2.6 to 4 metres in width. Later, a wooden extension of about 80 metres was added in 1892 to its seaward end in different orientation. (*Annex A2-3*) The historical map (Kowloon City Survey, Sheet No. 5, 1905) showed the Lung Tsun Pavilion/Pavilion for Greeting Officials located in the landward end, 20 supporting pillars, a Pier End Structure, and the extension part. (*Annex B1*) Historical photos showed that the decking of southern part was laid with 5 granite slabs, the supporting pillars were in hexagonal shape, and the decking pattern of its northern was different from its southern part. (*Annex C1-2*)

2.1.2 The Site was partly buried by Kai Tak Bunds in 1920s. The historical map in 1930s revealed most of the Bridge buried by the Kai Tak Bund, its wooden extension no longer existed, and the new structures added to its seaward end. (*Annex B3*)

2.1.3 The Site was completely buried by Japanese Army in 1940s to extend the Kai Tak airfield. Before the relocation of former Kai Tak Airport, the northern part of this Stone Bridge was overlaid by Terminal Building and its southern part was buried underneath the north apron of Kai Tak Airport. (*Annex B7-9*)

### 2.2 Statutory Designations

2.2.1 The Kai Tak Outline Zoning Plan (OZP) No.S/K22/2 was approved in 2007 with an Explanatory Statement stating that the Longjin Bridge was one of the archaeological sites in the Kai Tak area and its preservation condition as well as mitigation measures should be determined subject to the result of the archaeological impact assessment<sup>1</sup>.

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1 Paragraph 11.1 in the Explanatory Statement of Kai Tak OZP No. S/K22/2.

[http://www.ozp.tpb.gov.hk/pdf/s\\_k22\\_2\\_e.pdf](http://www.ozp.tpb.gov.hk/pdf/s_k22_2_e.pdf) .

2.2.2 The Kai Tak Development EIA report, displayed for public inspection in December 2008 and approved in March 2009<sup>2</sup>, recommended to preserve the extant sections of this Stone Bridge *in-situ* after further investigation and to draw up a CMP so that the identified sections of this Stone Bridge could be properly conserved and integrated into the future Kai Tak Development.

2.2.3 The site of Lung Tsun Stone Bridge has not been declared as monuments under the Antiquities and Monuments Ordinance (Cap 53).

### 2.3 Historic Development

The construction, use and burial of the Lung Tsun Stone Bridge were closely associated with the development of Hong Kong's history:-

#### Kowloon Street and Kowloon Walled City

2.3.1 The Kowloon Street, a market in the coastal area of Kowloon Bay probably existed since the late Ming dynasty, became much flourishing after the Opium War. Just as the presence of British in Hong Kong, the Kowloon Walled City was proposed in 1843 and completed in 1847 in order to reinforce the Chinese civil and military establishment at Kowloon. Moreover, Lung Tsun/*Longjin* Free School (龍津義學) was built in the same year at the Kowloon Walled City to strengthen inhabitants' moral standards against Western materialism. (*Annex A1*)

2.3.2 Prior to the construction of Lung Tsun Stone Bridge, a short wooden pier was probably built to meet the increasing needs of gambling houses fronting the beach, the merchants in the market and the officials and navy of the Kowloon Walled City.

#### Lung Tsun Stone Bridge and Lung Tsun Pavilion/Pavilion for Greeting Officials

2.3.3 A long stone landing pier with 21 supporting pillars was completed in 1875 after two years of construction. It was built like a traditional bridge in *Guangdong* area and called the Lung Tsun Stone Bridge(龍津石橋). Taking the pier example of China Merchants Company (招商局, *ZhaoShangJu*), a wooden extension was added in 1892 to the seaward end of the landing pier

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<sup>2</sup> Kai Tak Development, Application No. EIA-157/2008 in EIAO web page  
<http://www.epd.gov.hk/eia/english/register/aeiara/all.html>.

as the beach silted up and the Bridge was not suitable for cross harbour ferries at low tides. (*Annex A2-3*)

2.3.4 The Lok Sin Tong(樂善堂), a charitable organization which had been established in the Kowloon City market in 1880, played the leading role in the fund-raising of \$1,700 for the bridge extension. In return, each ferry paid monthly rental to the Lok Sin Tong.

2.3.5 At the landward end of this Stone Bridge was a two-storey pavilion, called the Lung Tsun Pavilion(龍津亭). The pavilion was used by the local elders to greet new officials to the *yamen* (Chinese court) in Kowloon City, and it was also called the Pavilion for Greeting Officials (接官亭). A stone lintel of 1873 inscribed with two large Chinese characters (龍津 Lung Tsun/*Longjin*) was placed in the main entrance of the Pavilion but no direct evidence was available to confirm when the pavilion was first built or any renovation. (*Annex C1-4*)

*Convention between Great Britain and China in respect of An Extension of Hong Kong Territory*

2.3.6 According to the Convention signed by China and British on 9 June 1898, the Kowloon Walled City was to remain under Chinese jurisdiction and the existing landing place near Kowloon Walled City should be reserved for the convenience of Chinese men-of-war, merchant and passenger vessel, which might come and go and lie there at their pleasure; and for the convenience of movement of the officials and people within the Kowloon Walled City. (*Annex A4-5*)

2.3.7 Since the colonial government encountered strong resistance to its occupation in New Territories in early 1899, the Governor hastily arranged flag hoisting ceremony and delivered a speech at the small open space in front of the Chinese custom station (only a few yards from the beach) on 17 April 1899. After the fights in Tai Po and Yuen Long, British troops were sent to force all the civil and military officials departing the Kowloon Walled City on 19 May 1899. Subsequently, the Order-in-Council was issued to legalize British jurisdiction on the Kowloon Walled City in December 1899. However, the issue of Lung Tsun Stone Bridge (or Kowloon City Pier) was not mentioned in this document. (*Annex A6-7*)

Kowloon City Public Pier and Kai Tack Bund

2.3.8 Setting aside the diplomatic arguments over the jurisdiction of the Kowloon Walled City, the Hong Kong colonial government immediately repaired the Kowloon City Pier (i.e. Lung Tsun Stone Bridge 龍津石橋) at a cost of \$5,829 in 1900 and arranged regular maintenance before the outbreak of 2<sup>nd</sup> World War. Moreover, the new pier facilities were constructed from 1910 to 1936-37 respectively. Information extracted from the relevant *Report of Director of Public Works* and other government records (*Annex D*) is listed out in the below:-

**Table 1**

<i>Year</i>	<i>Works for Kowloon City Pier (1900-1937)</i>	<i>Expenditure (HK\$)</i>
1900	Renewal of Kowloon City Pier completed	5,829
1910	<b>Erection of a new pier in Kowloon City completed in October and dredging of 21,608 cubic yards</b>	34,146.08 (partial)
1916	Renewing reinforced concrete beams and repairing masonry piers	601.39
1921	Maintenance	1,046.97
1922		Amount not available
1923	The decking of Kowloon City Pier badly damaged and general repairs	585.93
1925	Maintenance	Amount not available
1926		1,710.00
1927		399.79
1930	Rewiring, navigation light and general repair	5,408.92 (partial)
1931	Maintenance	1,725.04 (partial)
1934	Raising level of causeway to Kowloon City Pier	1,236.34
1936-37	<b>Reconstruction of Kowloon City Pier</b> ( Size: 80' x 32')	12,638.57

2.3.9 The Kai Tack Land Development Company commenced reclamation at Kowloon Bay in 1916 and completed the 1<sup>st</sup> stage development in 1924.

According to the historical map in 1930s, there were 6 original supporting pillars and pier end structure still existed outside the seawall of Kai Tack Bund. Moreover, the Bridge's timber extension disappeared, but the exact year of its demolition was uncertain. (*Annex B2 & B4*)

2.3.10 In 1920s, the Pavilion was demolished to make way for the constructing the new roads probably Sai Kung Road, Nga Tsin Wai Road and Prince Edward Road. The two stone tablets originally kept in the Pavilion were relocated in a small garden nearby the Police Station of Sai Kung Road together with the cannons of Kowloon Fort. They were lost during the Japanese Occupation period but the texts of these inscriptions were recorded.

2.3.11 Following the bankruptcy of Kai Tack Land Development Company, the Hong Kong colonial government took over the development project and designated the eastern part of the Kai Tack reclamation as the airfield for Royal Air Force (RAF) in 1925. The first control tower and a hanger were built in 1935. (*Annex B5-6*)

#### Japanese Occupation Period

2.3.12 Soon after the Japanese capture of Hong Kong in 1941, the new Japanese military government underwent rapid modification to enhance facilities in the airport for use of the Japanese Air Force planes. All the tenement buildings and shop houses of Kai Tack Bund were demolished and the villages around the area (today's San Po Kong) were also eradicated by the Japanese. In addition to the debris came from the above site clearance, rock from Sacred Hill, Hammer Hill and Po Kong Village *fung shui* hill as well as the stone walls of Kowloon Walled City were also used as fill materials for airfield extension.

2.3.13 The Japanese built two new runways and used the new airport solely as a military airfield from 1943 to 1945. Since then, the remaining parts of Lung Tsun Stone Bridge and the new pier structure were entirely covered by fill materials. (*Annex B7*)

#### Kai Tak International Airport

2.3.14 After the 2<sup>nd</sup> World War, the colonial government re-opened the Japanese military airport as an RAF airfield in 1945 and the civilian airport

was operated again in 1946. In 1947, the unsatisfactory seafront of the Japanese reclamation was reinforced by a newly constructed seawall across its front. Reclamation for the new runway started in 1956, and the new runway was opened to traffic in 1959. The history of Kai Tak Airport was the continual expansion and development until the Site reached full capacity. For instance, the Terminal Building was extended five times, twice for the runway, and at least three times for the apron. Hence, a number of works for former Terminal Building and associated facilities would have disturbed the underground Bridge remains from 1950s to 1990s. (*Annex B8-9*)

2.3.15 In view of the archaeological findings recovered from the Kowloon Walled City in 1994, the possibility of discovering remains of Lung Tsun Stone Bridge below the foundation of Terminal Building and North Apron could not be completely ruled out. After relocation of Kai Tak Airport in 1998, archaeological investigation was required to ascertain the remains of *Longjin* Bridge in the *Southeast Kowloon Development* project and *Kai Tak Development* project respectively.

2.3.16 Below is a summary timeline and relevant information of the Site for reference (*Table 2*).

1847	Completion of Kowloon Walled City and <i>Longjin</i> Free School
1873-75	Construction of Lung Tsun Stone Bridge
1892	Extension of Lung Tsun Stone Bridge
1899	Sir Henry Blake's speech to the elders of the villages of the Kowloon area, on 17 April, in front of the Chinese Customs Station, only a few yards from the beach, outside Kowloon City.
1899	Expulsion of Chinese Civil and Military Officials from the Kowloon Walled City on 19 May
1900	Renewal of Kowloon City Pier was completed
1910	Erection of a new pier in Kowloon City was completed in October (149 ft long, with an area of 2,950 sq ft)
1916-20	1 <sup>st</sup> stage of Kai Tak Reclamation
1920s	Demolition of Lung Tsun Pavilion and Lung Tsun Stone Bridge partly buried
1927-30	Construction of Kai Tak Airfield
1935	First control tower and a hangar were built

1936-37	Reconstruction of Kowloon City Pier
1942-45	Construction of airport runways during the Japanese Occupation period
1943	The walls of Kowloon Walled City were demolished to provide fill material for Kai Tak Airport extension
1957	Completion of Kai Tak Airport Reclamation
1958	Construction of a new northwest/southeast heading 2529 meter runway on a promontory into Kowloon Bay
1959	Completion of expansion of Hong Kong International Airport
1962	A passenger terminal building was built
1965	Demolition of the old Kai Tak Terminal Building
1974	Completion of Airport Runway extension
1975	Extension of Airport runway to 3,390 meters
1981	Completion of Stage 4 development of the passenger terminal building in 1981
1984-88	Stage 5 development of the passenger terminal building
1998	Relocation of Kai Tak Airport

## 2.4 Archaeological Investigations

2.4.1 According to the research papers of Walter Schofield and James Watt, some pre-Han dynasty pottery sherds and ceramic sherds of Song dynasty had been probably collected at Sacred Hill.

2.4.2 An archaeological investigation was conducted to ascertain any remains of Kowloon Walled City from 1993 to 1994 after the site clearance. Two stone tablets engraved with “ South Gate”(南門) and “ *Jiu long Zhai cheng*” (九龍寨城, i.e. Kowloon Walled City) were discovered at the location of the South Gate. The site of South Gate was later declared as a statutory archaeological site in 1996 and integrated into the park for public viewing<sup>3</sup>.

2.4.3 In the archaeological investigation for *Comprehensive Feasibility Study for the Revised Scheme of South East Kowloon Development*, no remains of

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3 古物古蹟辦事處 2007 《香港九龍寨城發掘簡報》，《考古》，2007年第6期，頁30-35。

archaeological interests were identified in 2002. Prior to the demolition of the former Terminal Building, another investigation was carried out at North Apron of former Kai Tak Airport in 2003 for the project of *Southeast Kowloon Development*. No evidence of the Lung Tsun Stone Bridge was identified but the alignment of the 1924 seawall provided an important clue for further investigation.

2.4.4 In 2006, AMO arranged a Watching Brief for the box culvert of the project namely *Preparation and Drainage Works for South East Kowloon Development Site at North Apron Area of Kai Tak Airport* at the possible area of Lung Tsun Stone Bridge landward end and Pavilion for Greeting Officials. Some wooden piles, isolated granite slabs and pottery sherds were identified in several sections of the drainage alignment.

2.4.5 In the EIA Study for KTD, an archaeological investigation was conducted from April to June 2008. The remains of the southern section of Lung Tsun Stone Bridge were first discovered in Test Trench AA5. Following the recommendation of EIA report, a further investigation was completed in February 2009. Several sections of the northern part of Lung Tsun Stone Bridge as well as the three foundation walls of the Pavilion for Greeting Officials were found at a level below the basement floor of former Terminal Building. The 47 broken concrete supporting pillars and landing steps of the former Kowloon City Pier were also discovered at the open area of former North Apron. Given the layout of Kowloon City Pier recorded on the 1930s map (*Annex B3*), no evidence of 1892 timber extension could be obtained from 2008-09 excavations. Conceivably, the complete clearance of timber extension was to ensure safe navigation.

2.4.6 Major archaeological features of the Lung Tsun Stone Bridge and Kowloon City Pier identified at the Site by 2008-09 archaeological investigations are summarized (**Figure 1**) as follows:-

- 3 foundation walls of the Pavilion for Greeting Officials and structural parts of landward end,
- 4 sections of the northern part of the Bridge in different conditions,
- 9 supporting pillars (SP) of its southern part (only SP2 is completely preserved),

- 1 complete granite decking (GD1) between SP1 and SP2 and half span of granite decking (GD2) between SP2 and SP3,
- 2 concrete spans between SP7 and SP9 (CS1 & CS2),
- a stone Pier End Structure of Lung Tsun Stone Bridge,
- 47 broken concrete supporting pillars and landing steps of Kowloon City Pier (badly damaged by previous works or disturbance, no ground floor of pier could be identified),
- Segments of 1924 and 1930s seawalls.

For detailed measurements of all the identified features, please refer to the Further Archaeological Excavation Report (2009).

2.4.7 In general, all the archaeological features identified at the Site are currently in a stable condition. However, the loose fragment, such as individual or small granite blocks, concrete or sandy mortar, at the surface of the remains, will be affected by soil movement, storm water, surface runoff or ground water changes etc. Therefore, the structural assessment report of further archaeological investigation proposed several measures to preserve those loose fragments. The details of archaeological features and extent of Bridge's remnants shall refer to the archaeological excavations 2008 and 2009 reports.

## **2.5 Townscape and Landscape**

2.5.1 The Lung Tsun Stone Bridge and Pavilion for Greeting Officials were originally built at the seashore, linking with the Kowloon Street and the East Gate of Kowloon Walled City. They were prominent landmarks on the Kowloon coastline in the late 19<sup>th</sup> century. (*Annex C7-9*)

2.5.2 Since 1900, the Lung Tsun Stone Bridge had been maintained and modified by the Hong Kong colonial government as Kowloon City Pier. Owing to the Kai Tack Bund development, the Bridge was substantially buried and the Pavilion for Greeting Officials was demolished. Before the outbreak of 2<sup>nd</sup> World War, the original stone structures of the Bridge and the new concrete structures still served as a landing pier in the Victoria Harbour.

2.5.3 Owing to the airfield extension in 1942-43, the remaining parts of Lung Tsun Stone Bridge and new structures of Kowloon City Pier permanently disappeared in the coastline. Since the development of Kai Tack

Bunds and Kai Tak Airport, the site of Lung Tsun Stone Bridge has been separated from other areas of Kowloon City due to the development of new road network and airport control.

## **2.6 Ownership and Management**

2.6.1 The remains of the Lung Tsun Stone Bridge and other features identified by the archaeological investigation are located in the government land managed by Lands Department.

2.6.2 The stone lintel inscribed with“同治十三年甲戌仲夏 龍津 南海潘仕釗書”, probably a remnant of the pavilion, is currently found at the back of the Lok Sin Tong Primary School entrance in Kowloon City District. Another side of this lintel facing the Nam Kok Road is inscribed with“樂善堂”, (Lok Sin Tong). This stone lintel is not an antiquity under the Antiquities and Monuments Ordinance and therefore the Government is not vested with the ownership. (*Annex C5-6*)

## **2.7 Gaps in Our Knowledge**

2.7.1 The layout of Lung Tsun Stone Bridge relies solely on historic maps and the text from the stone tablets. There are gaps between the construction works and dates of these maps. (*Annex B1& B3, A2-3*) Archaeological excavations have revealed the extent of archaeological features relating to the Bridge and Kowloon City Pier. Judging from the layout on the historic maps, the structures of the original Lung Tsun Stone Bridge could be determined. However, the subsequent alterations or additions could not certainly be ascribed to any date.

2.7.2 The details about the supporting pillars and granite decking in its southern part have been recovered. Nonetheless, there is no sufficient evidence to determine the details of granite decking in its northern part, the actual dimensions and building details of the Pavilion except the little information available from some old photographs. Especially, some areas in the former Kai Tak Airport terminal building have not been investigated.

2.7.3 All the archival records about construction works for Kowloon City Pier would have been destroyed during the war. Therefore, no information is

available to illuminate the architectural or structural details of the former Kowloon City Pier .

### **3. ASSESSMENT OF SIGNIFICANCE**

#### **3.1 Introduction**

The following assessment on the heritage significance of Lung Tsun Stone Bridge is mainly based on the archaeological findings recovered by two excavations conducted between April 2008 and February 2009, and basic documentary research. The assessment addresses the historical, townscape and landscape, regional and territory, archaeological, architectural and cultural significance of Lung Tsun Stone Bridge, and attributes the level of significance to archaeological features.

#### **3.2 Historical Significance**

The Lung Tsun Stone Bridge is of high historical significance due to its emergence relating to the Chinese civil and military establishment as well as the local community in late 19<sup>th</sup> century, its strategic position to uphold Chinese jurisdiction on Kowloon Walled City in the Convention for extension of Hong Kong colony, and its burial associated with Kai Tack Bunds and the airport development during the Japanese Occupation period and colonial period.

The Lung Tsun Stone Bridge was built in 1873-75 after the Hong Kong colonial government annexed the Kowloon peninsular. The location for building a landing pier in such a scale was to facilitate both Chinese civil and military forces stationed in the Kowloon Walled City overseeing the City of Victoria in the Hong Kong Island. The bridge extension had been completed and funded by the Lok Sin Tong in 1892, illustrating the growth of local Chinese community and the vast demand of crossing harbour ferries. The Lung Tsun Stone Bridge facilitated daily operations carried out by the *Guangdong* Customs (常關) and later the Kowloon Customs (九龍關) of Chinese Maritime Customs(洋關). These operations included opium tax collection and operations against opium smuggling. After the conflicts in Tai Po and Yuen Long, the colonial government used these incidents as an excuse to uproot the Chinese government's establishment within the colony's territory, especially the operations of Chinese Maritime Customs.

The Chinese government purposely reserved its right on this landing pier in the *Convention between Great Britain and China in respect of An Extension*

of *Hong Kong Territory* so as to facilitate the Chinese officials and people entering the Kowloon Walled City. However, expulsion of Chinese officials in 1899 and immediate repair for the Lung Tsun Stone Bridge in 1900 clearly demonstrated the Chinese jurisdiction on the Kowloon Walled City was deprived and the Lung Tsun Stone Bridge was taken over as colonial property. Since then, the Lung Tsun Stone Bridge no longer served the Chinese officials of Kowloon Walled City but its importance to Chinese government was explicitly recorded in the Convention signed by the British and Chinese in 1898.

During the colonial period from 1900-41, the Lung Tsun Stone Bridge or Kowloon City pier still served as a landing pier in Kowloon. The seaward part of this stone bridge was partially maintained after the Kai Tack Bund development but the reclamation area has changed rapidly for the development of airfield since late 1920s. During the Japanese Occupation period, the Kowloon Walled City was torn down and the Lung Tsun Stone Bridge was completely covered for the purpose of airfield extension. After the 2<sup>nd</sup> World War, the Terminal Building and the North Apron of Kai Tak airport were built on the Site of the Lung Tsun Stone Bridge without concerning its presence on the previous coastline. The Site of Lung Tsun Stone Bridge could vividly illuminate the development of Kowloon and Kai Tak Airport against the colonial background of Hong Kong.

### **3.3 Townscape and Landscape Significance**

The Lung Tsun Stone Bridge was once an important landmark on the coastline, linking with the Kowloon Street and Kowloon Walled City. Before the extension of colony's territory, it was a visual reminder of the presence of Chinese Officials and navy. The archeological features identified in the existing burial context could represent the rapid change of townscape and landscape in Kowloon from 1875 to 1998.

### **3.4 Archaeological Significance**

The archeological features identified within the original site of Lung Tsun Stone Bridge are physical evidences relating to the Kowloon Walled City, Lok Sin Tong, Kowloon City pier, Kai Tack Bund and airport development during the colonial and Japanese Occupation periods. All these archeological features recovered are significant in terms of urban or historical archaeology

in Hong Kong. Similar to the archaeological discovery at the Kowloon Walled City, the significance of remnants relating to Lung Tsun Stone Bridge are remarkably high against the Hong Kong's colonial background.

### 3.5 Architectural Significance

According to the San On Gazetteer (《新安縣誌》) of *Kangxi* and *Jiaqing* editions (康熙及嘉慶版), there was a bridge called *Longjin* Bridge (龍津橋) built in Song dynasty beside the *Longjin* Pagoda located in the present *Shenzhen* (深圳) City<sup>4</sup>. Another stone bridge also called *Longjin* Bridge (龍津橋) was built with supporting pillars in hexagonal shape (i.e. *fen shui jian* 分水尖 or cutwater) in the *Qianlong* (乾隆) reign of Qing dynasty, located in the *Panyu* (番禺) county.<sup>5</sup> According to the information of historic bridges recorded in the *Guangdong Relics Map* (*Zhongguo wen wu di tu ji- Guangdong fen ce* 《中國文物地圖集廣東分冊》) published in 1989, more than a hundred of similar Qing dynasty stone bridges were built with supporting pillars in hexagonal shape located in various cities or counties of Guangdong area.

In local context, a bridge leading to Sacred Hill with two angular supporting pillars (cutwater) had been recorded on a 1930s map. It seemed to be made of stone and probably vanished during the Japanese Occupation period. (*Annex E1-4*) Moreover, four stone bridges with similar supporting pillars are recorded in the AMO's research files, namely Bin Mo Bridge (便母橋) built in the 49<sup>th</sup> year of the Emperor *Kangxi* (康熙49年, 1710) reign, Lee Tat Bridge (利達橋) built in the 29<sup>th</sup> year of the reign of Emperor *Guangxu* (光緒29年, 1903), old bridge in Lin Ma Hang (蓮麻坑) of Sha Tau Kok (沙頭角) probably constructed before 1913, and Wui Sin Bridge (會仙橋) built in 1926

4 《新安縣誌、康熙版》卷三地理志之梁：龍津橋，在歸德之西，石塔下。(Zhang, 2006a, p.83)《新安縣誌、嘉慶版》卷七建置略之梁：龍津橋，在歸德之西石塔下。(Zhang, 2006b, p.382)《新安縣誌、嘉慶版》卷十八勝蹟之古蹟：龍津石橋：在邑之三都沙井村河邊。宋嘉定年間，鹽大使建石橋于沙井之東北。橋成之日，波濤洶湧，若有蛟龍奮躍之狀，故立塔於上以鎮之。(Zhang, 2006b, p.631).

5《中國文物地圖集廣東分冊》之番禺縣，頁234(44-C24 龍津橋條)：石橋鎮大嶼村、清代、建於清康熙年間。東西向，長28.2、寬3.2米，為石砌墩式拱券平橋。橋墩兩端做成分水尖形，橋孔有橫排方洞，可置橫木防止船隻出入。橋面兩側沒有欄杆，上雕刻纏枝花、八寶和鯉魚跳龍門等圖案及草書“龍津”二字。(Guangdong Sheng Wen Hua Ting, 1989, p.234)

within Hong Kong's territory. (*Annex E5-11*) The Lung Tsun Stone Bridge and the small bridge leading to Sacred Hill were built in the area of Kowloon City, resembling the style of stone supporting pillar (cutwater) and craftsmanship of Qing dynasty stonework in *Guangdong* area.

The construction period for the original Lung Tsun Stone Bridge was two years (1873-75) to build two main parts: - the solid stone structure of its northern/landward part and the supporting pillars of its southern/seaward part. The construction process required the profound knowledge on the local tide movements. Structural details of supporting pillar and landward part could illustrate how its layout and design to meet the needs for longer spans and heavier load-bearing capacities in the sea. As time went on, remnants of the Lung Tsun Stone Bridge in Kai Tak Area and the 4 existing bridges in rural areas are the surviving examples of typical Qing stone bridge in Hong Kong. In fact, the Lung Tsun Stone Bridge was not an ordinary bridge but a landing pier adopting the style of stone cutwater (supporting pillar in hexagonal shape) commonly used in most river bridges of Guangdong province during the Qing dynasty. (Examples of similar river bridges in Shenzhen and Dongguan at *Annex E12-14*) Moreover, the Bridge comprised the traditional Chinese cutwater, pavilion and Pier End Structure with landing steps similar to other pre-war piers in Hong Kong. The stonework style of Pier End Structure at the Site is found heterogeneous from the unearthed supporting pillars (traditional Chinese cutwater). Further research could be arranged to reveal more about the architectural significance of this Pier End Structure by the architecture historian.

### **3.6 Cultural Significance**

The Site is an epitome of pre/post war development in Hong Kong. The remnants of Lung Tsun Stone Bridge, Kowloon City Pier and seawall of Kai Tack Bund & etc could provide physical evidence to illustrate how the bridge/pier was built by the Chinese, taken over by the colonial government, damaged by Japanese Army, and how the Site was associated with the airport development of the Hong Kong. Undoubtedly, the archaeological discovery in Kai Tak area would facilitate Hong Kong's citizen to understand cultural and social roots and to create a sense of continuity that is an essential part of cultural identity.

### 3.7 Levels of Significance

In addition to assessing the significance of the whole site, consideration has been given to the levels of significance of different elements/parts of Lung Tsun Stone Bridge including its later alterations and other features uncovered by the archaeological excavations. Levels of significance have been assigned to the archaeological features of the Site that could provide specific direction of priority in relation to *in-situ* preservation or active conservation work, and could indicate where there is greater or lesser scope for adaptation and alteration without diminishing the overall significance. Three categories are set in the levels of significance, namely high, medium and little significance.

#### 3.7.1 High significance

Archaeological features of high significance are those which are of individual significance and which contribute in a fundamental way to an understanding of the Lung Tsun Stone Bridge as a heritage place. They are features of historical importance or individual distinction, or features in relatively intact conditions as discovered.

Archaeological features of high significance include:

- 3 foundation walls of the Pavilion for Greeting Officials and structural parts of landward end,
- 4 sections of the northern part of the Bridge in different conditions,
- 9 supporting pillars (SP) of its southern part (only SP2 is completely preserved)+ ,
- 1 complete granite decking (GD1) between SP1 and SP2 and half span of granite decking (GD2) between SP2 and SP3,
- 2 concrete spans between SP7 and SP9 (CS1 & CS2),

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+ Note: There were 21 supporting pillars mentioned in the *Inscription of Lung Tsun Stone Bridge* in 1875 (Annex A2), but only 20 supporting pillars were shown in the 1905 Kowloon City Survey Sheet No. 5 (Annex B1). 9 supporting pillars were discovered in the archaeological investigations in 2008-09 (Figure 1) and the total number of supporting pillar is not yet confirmed as the central portion of the bridge was not fully exposed in the investigations.

- a stone Pier End Structure of Lung Tsun Stone Bridge.

### 3.7.2 *Medium significance*

Archaeological features of medium significance are those which originally were of a secondary nature or which may be later additions and alterations which are important in the evolving nature of the place. While not necessarily of individual distinction, they are important in contributing to the significance of the place as a whole. Retention of archaeological features of medium significance is preferred although there may be scope for preservation.

Archaeological features of medium significance are:-

- 47 broken concrete supporting pillars and landing steps of Kowloon City Pier

### 3.7.3 *Low significance*

Archaeological features of low significance, in general, are a recent structure contributes low significance to the place. Those features may be intrusive to the original elements of the Lung Tsun Stone Bridge.

Archaeological features of low significance are:-

- Segments of 1924 and 1930s seawalls

## **4. VULNERABILITES AND ISSUES**

### **4.1 Introduction**

This section will set out the vulnerabilities, key issues facing the Site and the existing problems that may need to be solved in order to find a long-term sustainable future of the Site. In the following Section 5 Opportunities, some of the issues and vulnerabilities identified here will also be discussed from the perspective of potential benefit associated with the new developments in the Kai Tak Area.

### **4.2 Inappropriate New Use will Jeopardize the Site's Significance**

In view of the Site's significance mentioned in previous section of this CMP, identifying a sustainable new use with appropriate interpretation for the Site is essential for the future. However, a proper balance shall be struck when considering the use for the Site and any construction works within the Site once the scope for *in-situ* preservation being defined.

### **4.3 Management, Maintenance and Monitoring Issue**

Following the recommendation of the Kai Tak EIA Report, the extent of Lung Tsun Stone Bridge remains determined by the further archaeological excavation will be preserved *in-situ*. The scope for *in-situ* preservation shall be defined and based on the levels of significance presented in previous section of this CMP. Nevertheless, the archaeological features identified are the remains of Lung Tsun Stone Bridge which could be preserved *in-situ* by reburial, partial reburial and display of selected elements, or full display of all archaeological features. No matter whether the reburial or display of this Bridge remains is selected, any proposed new works at the Site should consider the Heritage Impact Assessment issue with reference to the Technical Circular (Works) No. 6/2009 of Development Bureau and conduct assessment on such proposed arrangement be sustained economically or assessment on the same result of such proposed arrangement be achieved by other means.

In case of reburying all archaeological features, the issues of Site's management and monitoring shall be worked out to ensure its stability and integrity during and after the construction stage of Kai Tak Development. In case of displaying ruins in full or partial, the long term mechanism and cost

for long term management and maintenance shall be carefully considered before making any decision without adequate information. Hence, the management, maintenance and monitoring plans could be formulated by the future site managers in conjunction with the relevant government departments/agencies, authorities and stakeholders.

#### **4.4 Zoning Issue**

The Kai Tak OZP No.S/K22/2 covering Lung Tsun Stone Bridge was approved by the Chief Executive in Council in 2007. The archaeological excavation completed in February 2009 has identified the extent of Lung Tsun Stone Bridge remains, involving two commercial sites (i.e. site 2A1 of 7,000sq.m and 2A2 of 6,800sq.m including the Underground Shopping Street), and two linked residential areas (i.e. site 2B1 of 21,700sq.m), a pedestrian street of 10m wide and Road L8 on the approved OZP. **(Figure 12)** Under the approved OZP, the affected commercial and residential sites, subject to building height restrictions of 45-110mPD, will altogether provide a total domestic gross floor area (GFA) of 108,500sq.m and non-domestic GFA of 64,270sq.m.

In case that the existing zones for these areas deposited with the remnants of Lung Tsun Stone Bridge may remain unchanged, the future buildings and structures surrounding the Site will obviate the visual corridor for the Bridge's remnants. The planned land uses for these areas shown on the current approved OZP prior to the discovery of the remnants of Lung Tsun Stone Bridge have not reflected the planning intention to conserve the remnants. Any proposed changes on the existing "Commercial" and "Residential" zones or planned road etc will have cost and revenue implications to the public finance, notwithstanding preserving the remnants of Lung Tsun Stone Bridge could be regarded as social/public investment.

#### **4.5 Planned Infrastructure Issue**

In accordance with the approved Kai Tak OZP, the infrastructure projects associated with the Kai Tak Development have been designed and will be implemented through the existing statutory or administrative procedures. The discovery of Lung Tsun Stone Bridge and the recommendation of *in-situ* preservation in EIA report will have implications to the design and layout of planned infrastructure projects within that part of

the former north apron in Kai Tak Area. Moreover, other planned infrastructure projects of inter-regional or territory-wide importance, such as the Shatin-Central Link, may have impacts on the remnants of Lung Tsun Stone Bridge during their construction and operation stages.

#### **4.6 Level Difference Issue**

The Lung Tsun Stone Bridge and its concrete alterations/additions in 1910-30s had been buried due to previous reclamations. During the excavations, archaeological features of the Bridge were discovered at the level from +2.8 to -2mPD and the underground water level was found at the level about +2.5 to 3mPD. Due care should be taken in the course of construction works of future buildings/structures or public utilities/facilities surrounding the Site to avoid any adverse impacts on the existing burial environment of the Lung Tsun Stone Bridge or soil settlement, thus affecting its stability.

#### **4.7 Public Expectation Issue**

Given the wide media coverage on the discovery of Lung Tsun Stone Bridge in mid 2008 and early 2009, the public is generally aware that the Bridge remains may be preserved in the Kai Tak Development probably with appropriate site interpretation. Several local heritage concerned groups have expressed keen interests in conserving the Bridge remains. Even before the further archaeological investigation report is available for public inspection, these concerned groups have submitted proposals to make some substantial changes on the approved OZP and associated infrastructure projects. The diversified views from different stakeholders or sectors may take time to resolve the Site's future. Hence, the general public may expect some proposals from the relevant government agencies and the future managers of the Site in line with the current policy for heritage conservation.

## **5. OPPORTUNITIES**

### **5.1 Introduction**

As illustrated in the previous sections of this CMP, the Site of Lung Tsun Stone Bridge is of high significance and will have a number of issues and vulnerabilities in the Kai Tak Area. Simultaneously, this historic site can provide several beneficial opportunities in Kai Tak Development (KTD). This section will set out the opportunities available to conserve and sustain the Site for the future.

### **5.2 Integration into the Planning for Kai Tak Area**

In accordance with the Explanatory Statement of the Kai Tak OZP, the Kai Tak Area is proposed to be developed as the “Heritage, Green, Sports and Tourism Hub for Hong Kong”. As a result of the further archaeological excavations recommended by the approved KTD EIA Report, the extent of Bridge remains has been basically identified to facilitate the planning of Kai Tak Area. With reference to the levels of significance in section 3 of this CMP, the scope of extant Stone Bridge’s remains could be determined for *in-situ* preservation. With appropriate display facilities, the heritage and tourism values of the Site can be enhanced and the history of Kai Tak Area can be manifested as stated in the Explanatory Statement.

Moreover, “a heritage trail” winding through and connecting the Metro Park with Sung Wong Toi Park is proposed in the paragraph 7.2.8 of the Explanatory Statement of the Kai Tak OZP. It is worthwhile to consider incorporating the remnants of Lung Tsun Stone Bridge into the proposed heritage trail to create a sense of place and a sense of continuity.

### **5.3 Cultural Heritage Resource for Educational and Tourism Purposes**

There is an opportunity to illustrate the history of Lung Tsun Stone Bridge, colony’s territory extension and Kai Tak Airport through the remnants of Lung Tsun Stone Bridge. As a valuable cultural heritage resource in Hong Kong, the Site could be utilized for educational and tourism purposes. Subject to the scope for *in-situ* preservation and display, the Site could be designed with appropriate facilities during its planning stage to form an integral part of Kai Tak Area. Hence, the Site could be an excellent

historic place with a right degree of interpretation to the visiting tourist or school groups.

#### **5.4 Community Involvement**

The Heritage Conservation Policy Statement of Development Bureau explicitly states that the Policy shall be implemented with due regard to development needs in the public interest, respect for private property rights, budgetary considerations, cross-sector collaboration and active engagement of stakeholders and the general public. Given the Sites' significance presented in Section 3 and public expectation presented in paragraph 4.7 of this CMP, it is necessary to collect the public opinions on the future of the Site.

## 6. CONSERVATION GUIDELINES FOR THE SITE

### 6.1 Introduction

This section is intended to set out the guidelines for the future of the Site. Certainly, these guidelines can only act as principles and form a starting point for decisions about the future of the Site. The significance of the Site may also be changed over a period of time as the KTD is developed. It is important that this CMP in its present form is relevant to the situation in 2009. Hence, this CMP shall be regularly updated as changes are made to the Site and its surrounding area by KTD or by any infrastructure projects affecting the Site.

The following term definitions quoted from the *English Heritage Policy Statement on Restoration, Reconstruction, and Speculative Recreation of Archaeological Sites including Ruins* are used in this CMP.

**Restoration** means returning the existing fabric of a place to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material.

**Reconstruction** means returning a place to a known earlier state; distinguished from restoration by the introduction of new material into the fabric.

**Recreation** means speculative creation of a presumed earlier state on the basis of surviving evidence from that place and other sites, and on deductions drawn from that evidence using new materials.

### 6.2 Conservation Guidelines

A number of guidelines are proposed below as a framework to guide the conservation, management, maintenance, and interpretation for the future of the Site. The visions of these conservation guidelines for the Site are: -

- To protect and conserve the values and significance of the Site as the basis for all future planning and actions.
- To seek a balance between conservation and new uses of the Site to create a sustainable heritage place in Kai Tak Development.

***Guideline 1: The Site shall be recognized as a place of heritage significance with attributes embracing historic, cultural and architectural values.***

The Site of Lung Tsun Stone Bridge's heritage significance is demonstrated in the assessment in the previous sections of this document and should be formally acknowledged by those responsible for the Site. Ensure that the future managers of the Site recognize and understand the heritage significance of Site under their control. Review the guidelines for the Site before change/transfer of management or maintenance agents occurs and ensure that heritage management and maintenance requirements will fully implemented and monitored.

***Guideline 2: To apply the conservation principles and practices of the well-recognized charters and guidelines for preservation, restoration and reconstruction of the Site.***

The principles and guidelines of the *Nara Document on Authenticity* (1994), *Principles for the Recording of monuments, Groups of Buildings and Sites* (1996), *Burra Charter* (1979, revision of 1999), *English Heritage Policy Statement on Restoration, Reconstruction, and Speculative Recreation of Archaeological Sites including Ruins* (2001), *Principles for the Conservation of Heritage sites in China* (2003), *Hoi An Protocol for Best Conservation Practice in Asia* (2005), *Beijing Document* (2007), *Conservation Principles, Policies and Guidance for the Sustainable Management of the Historic Environment* (2008), but not limited to the above-mentioned, shall form the foundation for management of the Site of Lung Tsun Stone Bridge. The adoption will bring the Site into line with a widely accepted set of conservation principles and guidelines.

***Guideline 3: The levels of significance in this document forms the basis for any actions applicable to each category within the Site. To ensure that the Lung Tsun Stone Bridge shall be preserved with proper restoration and no speculative recreation is allowed at the Site.***

The levels of significance are given in Section 3 of this CMP. It represents a soundly based analysis and assessment on all available evidence to determine the scope of *in-situ* preservation, and to facilitate planning and maintenance decisions. Reconsider the significance of archaeological features

that may be uncovered in future during subsequent preservation/conservation works.

Archaeological features identified as having “high” significance in this document shall be preserved and interpreted. Archaeological features identified as having “medium” significance in this document should be preserved as the preferred option, but could be recorded by an archaeologist and removed if necessary. Otherwise, removal of any elements identified within the Site shall be restricted to intrusive elements on the Lung Tsun Stone Bridge or those of low significance.

**Table 3**

<i>Level of Significance</i>	<i>Recommendation for each category</i>
High significance	<i>in-situ</i> preservation required, conservation essential;
Medium significance	conservation desirable subject to the scope of preservation;
Low significance	retention or removal depending on other priorities; intrusive elements or features subject to removal or alteration to minimize adverse impacts on archaeological features of high significance.

Restoration is an intervention and shall be kept to minimal. Conjectural reconstruction for the Bridge and the Pavilion is not permitted at the Site. The Criteria for assessing proposals for restoration or reconstruction in *English Heritage Policy Statement on Restoration, Reconstruction, and Speculative Recreation of Archaeological Sites including Ruins* shall be adopted while considering any proposal for the Site.

***Guideline 4: To ensure that conservation works are documented appropriately and undertaken as necessary.***

The conservation, maintenance and new works shall be properly recorded. Ensure that conservation works are documented and supervised by a conservation architect or archaeologist. Contractors engaged to work on the Site will be suitably qualified and experienced in conservation techniques.

***Guideline 5: The management and maintenance of Lung Tsun Stone Bridge shall be in complete accord with the significance assessed in this CMP***

The Section 3 of this CMP gives overall direction regarding the significance of the archaeological features and setting/context with the level of significance. This CMP recognizes that heritage aspects of the Site must be balanced with other aspects such as use, financial constraints, and regulatory or legislative requirements in any future management or development. Conservation measures apply to archaeological features of high significance should be reversible. Interventions, alterations or additions will only be permissible where they reveal or reinforce the significance of the Site as a whole. The archaeological features of the Site will be cared for through a regime of cyclical preventative maintenance informed by a thorough condition survey.

An archive facilitating management and maintenance for the Site will be deposited with and maintained by the Antiquities and Monuments Office. This shall include copies of any statutory applications, reports of any archaeological excavations, recording works or watching briefs relating to the Site, any investigation reports of conservation plans or feasibility studies, records of any repair and consolidation works, records of any elements/structures erected or demolished within the Site, any photographic or drawn surveys of any part of the Site.

***Guideline 6: The interpretation for Lung Tsun Stone Bridge shall be based on the significance assessment in this CMP.***

The significance assessment in this CMP illustrates the significance which make up the importance of the Site, and form a useful means of enabling this importance to be communicated to visitors. The principles in the *ICOMOS Charter on the Interpretation and Presentation of Cultural Heritage Sites (2008)* shall be adopted for the Site's interpretation. Another excavation to expose all the features of Lung Tsun Stone Bridge and Pavilion of Greeting Officials should be considered before determining the scale of display and interpretation plan for the Site if necessary, since the archaeological investigations conducted from 2008 to 2009 were to determine the extent of archaeological remains.

***Guideline 7: Any future development projects or the planned infrastructure projects encroaching/affecting the Site shall not adversely affect the Site's significance.***

These guidelines recognize the inter-relationship between the Site of Lung Tsun Stone Bridge and the broader Kai Tak Area. The necessity of Heritage Impact Assessment for any new development or infrastructure projects should be considered to assess any potential impact on the heritage significance of the Site, and to design and implement the appropriate mitigation measures safeguarding the Site's significance.

***Guideline 8: The remains of Lung Tsun Stone Bridge built in 1875 are assessed of high significance and the appraisal for legal protection should be carried out.***

The Section 3 of this CMP is a foundation for relevant authorities and government departments to consider the protection issue of the Site such as the statutory procedures of the Antiquities and Monuments Ordinance (A & M Ordinance, Cap. 53) to provide legal protection for the Site, similar to the South Gate of Kowloon Walled City in 1996. The archaeological features of Lung Tsun Stone Bridge and Pavilion for Greeting Officials are of high significance and therefore might be considered to declare as a monument.

## 7. CONCLUSION

The Lung Tsun Stone Bridge was a landing pier built in 1873-1875 and its timber extension built in 1892 was dismantled in early 20<sup>th</sup> century. This stone bridge not only signified economic growth of that area in Kowloon, but also experienced the change of sovereignty due to the extension of colony's territory in 1898. The *Convention between Great Britain and China in respect of An Extension of Hong Kong Territory* was the key historical document to demonstrate its strategic position to uphold Chinese jurisdiction on Kowloon Walled City. Moreover, the Site is an epitome of the Kai Tak Airport i.e. the development of Kai Tack Bunds, airport extension during Japanese Occupation Period, and the postwar continual developments. The Site is of heritage significance with attributes embracing historical, cultural and architectural values. Undoubtedly, the archaeological discovery in Kai Tak Area could provide physical evidences for Hong Kong's citizen to understand cultural and social roots and to create a sense of continuity that is an essential part of cultural identity.

Regarding the archaeological features relating to Lung Tsun Stone Bridge discovered at the Site, the Section 3 of this CMP gives overall assessment with the levels of significance for sake of demarcating the scope of *in-situ* preservation. The remnants of original Lung Tsun Stone Bridge shall be preserved *in-situ*. The Site shall be properly conserved, maintained, interpreted and presented its heritage values and significance. No speculative recreation or excessive intervention would be allowed at the Site in accordance with the widely accepted conservation principles and guidelines.

This CMP recognizes that heritage aspects of the Site must be balanced with other aspects such as use, financial constraints, and any future needs of KTD. The Conservation Guidelines of this CMP are a series of statements of what should and what should not be done to the Site. The intention is that all these guidelines should be understood and endorsed by relevant government departments/agencies, authorities and stakeholders as well as the future Site managers who will take up the future site management and interpretation and update this CMP as necessary.

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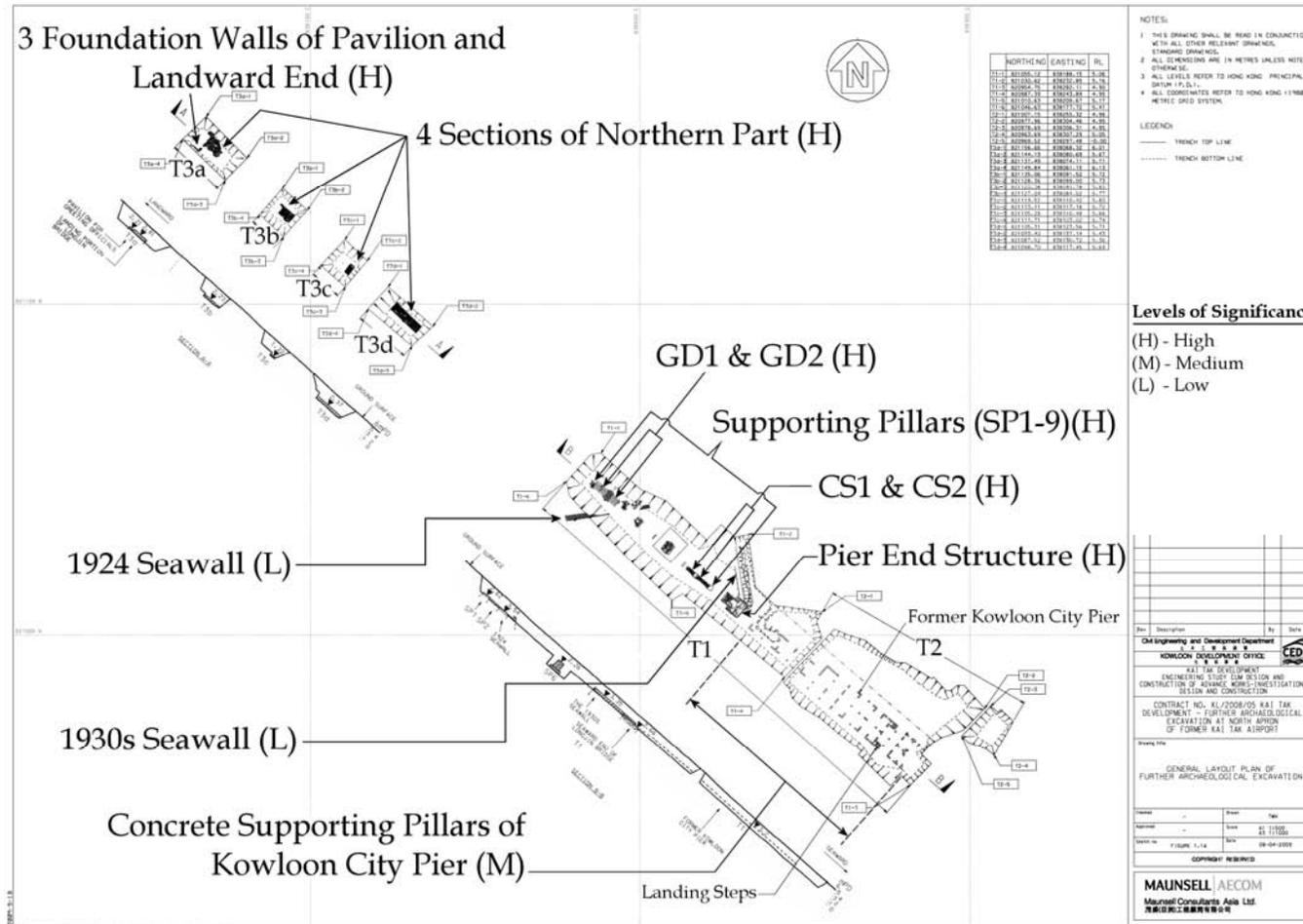


Figure 1 Major archaeological features identified at the Site by 2008-09 archaeological investigations with levels of significance



**Figure 2** Foundation walls of Pavilion and northern part of the stone bridge in Trench T3a



**Figure 3** Northern part of the stone bridge in Trench T3b



**Figure 4** Northern part of the stone bridge in Trench T3c



**Figure 5** Northern part of the stone bridge in Trench T3d



**Figure 6** Supporting pillar and granite decking  
(From right to left: SP1, GD1, SP2, GD2)



**Figure 7** Supporting pillar SP6



**Figure 8** Supporting pillar SP6 and its footings



**Figure 9** Pier End Structure and 1930s seawall



**Figure 10** Concrete supporting pillars of Kowloon City Pier  
(from SE to NW)



**Figure 11**  
Concrete supporting  
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## Textual Records

- 1a. 九龍司新建龍津義學敘 (1847)  
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6. 1899 年 12 月 27 日英國所頒佈的「樞密院令」  
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7. 港督卜力於 1899 年 4 月 17 日九龍城發表的演辭  
The Speech of the Governor, Sir Henry Blake, delivered at Kowloon City on 17 April 1899

## 37. 九龍司新建龍津義學叙\*

九龍寨城龍津義學

有因時制宜者出，相讓勢，備經營，即事求治，而招撫懷遠之意以寓。蓋經濟之才，如此其難也。粵東素稱樂土，人文與中州相埒，貨財之所萃會，資帛之所駢集，富庶又甲於他省。新安特濱海邊。邑縣有官富司，尤濱海邊司耳。然衣之繭曰邊，器之羨曰邊。器敵自羨始，衣敵自繭始。則凡官邊地者，皆共厥職，宜什伯中土；而賦薄之，獨何心歟？道光二十三年，夷務靖後，大吏據情入告，改官富為九龍分司。由近置移於遠，築城建署，聚居民以實之。雖備內，不專為禦外，而此中稟承 廟謨，計安海宇，誠大有濟時之識於其間，而非苟為勞民傷財也。今年余奉調視事，巡檢許君文深來言，有龍津義學之建。副將王君鵬年，通判顧君炳章，喬大令應庚及許君捐銀若干為經始，地租歲可得若干以資生徒，仿古家師之制，擇其尤者居焉。人心胥奮。嗟乎。此真即事求治，能以無形之險，固有形者也。今 國家文教覃敷，武功赫耀，無遠弗屆。九龍民夷交涉，人情重貨實而薄詩書，有以鼓舞作興，則士氣既伸，而外夷亦得觀感於絃誦聲明，以柔其獷悍之氣。所為漸被邊隅者，豈淺鮮哉？落成，司人以文請。既滋愧許君能助我不逮，而重為司人深無窮之望也。記之，俾勸於石。道光丁未秋八月知新安縣事思唐七銘鼎撰 南海謝鐵筆刻

(原碑未見，據許舒博士藏抄本編錄)

九龍司新建龍津義學叙，1847

Inscription of Lung Tsun Free School, 1847

(Faure, Luk, &amp; Ng, 1986, p.101)

其猶龍乎卜他年鯉化蛟騰盡洗蠻烟蛋雨  
是知津也願從此源尋流溯平分蘇海韓潮

龍津義學門聯

Couplet on the granite door frames at the entrance of Lung Tsun Free School

(Zhou, &amp; Zhong, 2001, p.77)

## 61. 龍津石橋碑

九龍寨城

新安地瀕遐海，九龍山翠，屏峙南隅。環山居者，數十萬家。自香港埠開，肩相摩、踵相接。估船番舶，甲省東南。九龍趁集日夥；蟻人操舟，漁利橫流，而渡無虛期。地沮洳阻深，每落潮，篙師無所逞。同治歲癸酉，衆釀金易渡而梁。計長六十丈，廣六尺，爲礫二十有一。糜金錢若干。光緒乙亥橋竣。夫徐道成梁，古王遺軌。然工程全集，往往道潰於成。謀夫孔多，職此之咎。今都人士，一乃心力，以告厥成功。使舊時激湍之區，成今日津梁之便。垂之綿遠，與世無窮。此豈關地之靈歟？抑亦由人傑也。銘曰：叱鼉橫漢，駕鶴凌宵。在天成象，在地成橋。擲杖虹飛，受書溪曲。仰桂攀丹，垂楊撲綠。斬蛟何處，騎虎誰人。高車駟馬，於彼前津。石昏神鞭，杵驚仙搗，乘鯉江泉，釣鯨烟島。帽簷插杏，詩思吟梅。風人眺覽，雲客裘裘。緊彼雖寬，臨江炫彩。矧此滄溟，樓船出海。乃邀郢匠，乃命僮工。紐牽怪石，斤運成風。投馬完隄，斷鼉支柱。未雲何龍，屹立江滸。兩湖碧暈，鹹沙珠圓。漁燈掩月，螢火沉烟。黃竹肩箱，綠荷包飯。彼往經營，此來負販。鬪橈蕩浪，桂棹凌波。震天水調，月夜漁歌。陵谷雖遷，滄桑不改。繫於金湯，萬年斯在。

欽加道銜安徽廬州府知府，署鳳穎六泗兵備道，前掌京畿道、江南道、湖廣道監察御史、山西提督學政洗斌補行己未壬戌恩科舉人南海何又雄書

倡建首事

廣東大鵬協鎮都督府彭

廣東大鵬協鎮中軍都關府劉

署新安縣九龍分司巡政廳周

光緒元年歲次乙亥孟冬吉日立

（原碑未見。今據《九龍樂善堂特刊》、《文匯報》八〇年五月十四日「香港街坊志」及黃佩佳《香港新界風土名勝大觀》互校載錄。）

龍津石橋碑 (1875)

Inscription of Lung Tsun Stone Bridge, 1875

(Faure, Luk, &amp; Ng, 1986, pp.166-167)

## 85. 龍津石橋加長碑

九龍寨城

天下事有致力於此，而收效輒及於他事者。其機不數觀。要惟好行方便者，往往得之。九龍濱海龍津石橋，創於同治癸酉，問津者咸便利之。顧地爲巨浸所朝宗，潮汐往來，沙積多停蓄。自成橋後，歲月積漸，滄桑改觀。邇來橋之不逮於水者，殆猶今之視昔焉。於是商於是地者，謀所以善其後也。乃仿招商局碼頭之制，續作橋廿四丈；又於其端，爲丁字形，寬一丈二尺。其製精而其費較省。且易石而木，泊船時亦無兩堅激撞之患，其爲用亦更適。計糜題捐洋銀一千七百有奇。至渡港小輪船以斯橋之利其載運也，每船願月輪碼頭租銀若干。會樂善堂施濟所需，捐款不恆，至僉碼頭租款宜屬之樂善堂，永資挹注。蓋藉斯地之財，即以濟斯地之用，實一舉而兩善具焉。昔莊子有言：以鹵莽耕者，天即以鹵莽應之。茲則以方便行者，天非以方便應之。人事所感，即天心所錄，斯可以識其大凡矣。是不可以不記。且爲之銘曰：長虹飲川，渡源雲屬。余木爲樑，用撈鼈足。如雁齒之平，匪鼻脛之續。資沾漑夫善堂，樂斯人之所欲。合藏市以出塗，慰成功以相告……光緒十八年歲次壬辰仲秋吉旦。

（原碑未見。引自《文匯報》八〇年五月十六日「香港街坊誌」。部份內容亦見於《九龍樂善堂特刊》。）

龍津石橋加長碑 (1892)

Inscription about extension of Lung Tsun Stone Bridge, 1892

(Faure, Luk, &amp; Ng, 1986, p.251)

## APPENDICES

I. CONVENTION BETWEEN GREAT BRITAIN AND CHINA  
RESPECTING AN EXTENSION OF HONG KONG TERRITORY  
SIGNED AT PEKING, 9TH JUNE, 1898

[Ratifications exchanged at London, August 6th 1898.]

WHEREAS it has for many years past been recognized that an extension of Hong Kong territory is necessary for the proper defence and protection of the Colony.

*Enlargement of British Territory under lease*

It has now been agreed between the Governments of Great Britain and China that the limits of British territory shall be enlarged under lease to the extent indicated generally on the annexed map. The exact boundaries shall be hereafter fixed when proper surveys have been made by officials appointed by the two Governments. The term of this lease shall be ninety-nine years.

*Jurisdiction*

It is at the same time agreed that within the city of Kowloon the Chinese officials now stationed there shall continue to exercise jurisdiction except so far as may be inconsistent with the military requirements for the defence of Hong Kong. Within the remainder of the newly-leased territory Great Britain shall have sole jurisdiction. Chinese officials and people shall be allowed as heretofore to use the road from Kowloon to Hsinan.

*Use of landing place near Kowloon by Chinese*

It is further agreed that the existing landing place near Kowloon City shall be reserved for the convenience of Chinese men-of-war, merchant and passenger vessels, which may come and go and lie there at their pleasure; and for the convenience of movement of the officials and people within the city.

*Appendices*

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*Railway*

When hereafter China constructs a railway to the boundary of the Kowloon territory under British control, arrangements shall be discussed.

*No expropriation or expulsion of natives*

It is further understood that there will be no expropriation or expulsion of the inhabitants of the district included within the extension, and that if land is required for public offices, fortifications, or the like official purposes, it shall be bought at a fair price.

*Extradition*

If cases of extradition of criminals occur, they shall be dealt with in accordance with the existing Treaties between Great Britain and China and the Hong Kong Regulations.

*Use of Mirs Bay and Deep Bay by Chinese Ships of War*

The area leased to Great Britain, as shown on the annexed map, includes the waters of Mirs Bay and Deep Bay, but it is agreed that Chinese vessels of war, whether neutral or otherwise, shall retain the right to use those waters.

*Ratifications*

This Convention shall come into force on the 1st day of July, 1898, being the 13th day of the 5th moon of the 24th year of Kuang Hsi. It shall be ratified by the Sovereigns of the two countries, and the ratifications shall be exchanged in London as soon as possible.

Done at Peking in quadruplicate (four copies in English and four in Chinese) the 9th day of June, in the year of our Lord one thousand eight hundred and ninety-eight, being the 21st day of the 4th moon of the 24th year of Kuang Hsi.

Claude M. Macdonald  
Li Hung-chang } Members of  
Hsu Ting-kuei } Tsungli Yamen

SOURCE: *Laws of Hong Kong*, 1964 Ed., Vol. 20, Appendix IV, p. 11.

「展拓香港界址專條」之英文本 (1898)

Convention for the extension of Hong Kong, 1898 - English version

(Sayer, 1975, pp.130-131)

一八九八年六月九日，光緒二十四年  
四月二十一日，北京。

溯查多年以來，案悉香港一處非展拓界址不足以資保衛，今中、英兩國政府議定大略，按照黏附地圖，展擴英界，作為新租之地。其所定詳細界線，應俟兩國派員勘明後，再行畫定。以九十九年為限期。又議定，所有現在九龍城內駐紮之中國官員，仍可在城內各司其事，惟不得與保衛香港之武備有所妨礙。其餘新租之地，專歸英國管轄。至九龍向通新安陸路，中國官民照常行走。又議定，仍留附近九龍城原舊碼頭一區，以便中國兵、商各船、渡艇任便往來停泊，且便城內官民任便行走。將來中國建造鐵路至九龍英國管轄之界，臨時商辦。又議定，在所展界內，不可將居民迫令遷移，產業入官，若因修建衙署、築造砲台等，官工需用地段，皆應從公給價。自開辦後，遇有兩國交犯之事，仍照中、英原約、香港章程辦理。查按照黏附地圖所租與英國之地內有大鵬灣、深圳灣水面，惟議定，該兩灣中國兵船，無論在局內、局外，仍可享受。

此約應於畫押後，自中國五月十三日，即西曆七月初一號開辦施行。其批准文據應在英國京城速行互換。為此，兩國大臣將此專條畫押蓋印，以昭信守。此專條在中國京城繕立漢文四份、英文四份，共八份。

大清國太子太傅文華殿大學士一等肅毅伯李，經筵講官禮部尚書許  
大英國欽差駐劄中華便宜行事大臣寶  
光緒二十四年四月二十一日  
西曆一千八百九十八年六月初九日

「展拓香港界址專條」之中文本 (1898)

Convention for the extension of Hong Kong, 1898 - Chinese version  
(Leung, 1995, p.254)

## AT THE COURT AT WINDSOR

The 27th day of December, 1899

PRESENT.

THE QUEEN'S MOST EXCELLENT MAJESTY

HIS ROYAL HIGHNESS THE DUKE OF CONNAUGHT AND STRATHEARNE

MR. BALFOUR

MR. RITCHIE

Whereas by a Convention dated the 9th day of June, 1898, between Her Majesty and His Imperial Majesty the Emperor of China, it was provided that the limits of British territory in the regions adjacent to the Colony of Hong Kong should be enlarged under lease to Her Majesty in the manner described in the said Convention.

And whereas by an Order of Her Majesty in Council, dated the 20th of October, 1898, it was, amongst other things, ordered that the territories within the limits and for the term described in the said Convention should be and the same were thereby declared to be part and parcel of Her Majesty's Colony of Hong Kong, in like manner and for all intents and purposes as if they had originally formed part of the said Colony, and it should be competent for the Governor of Hong Kong, by and with the advice and consent of the Legislative Council of the said Colony, to make laws for the peace, order, and good government of the said territories as part of the Colony.

And whereas by Article 4 of the said Order in Council it was provided that, notwithstanding anything in the said Order in Council contained, the Chinese officials at the date of the said Order in Council stationed within the City of Kowloon should continue to exercise jurisdiction therein except in so far as might be inconsistent with the military requirements for the defence of Hong Kong.

And whereas the exercise of jurisdiction by the Chinese officials in the City of Kowloon having been found to be inconsistent with the military requirements for the defence of Hong Kong, it expedient that Article 4 of the said Order in Council should be revoked, and that the Chinese officials within the City of Kowloon should cease to exercise jurisdiction therein, and that the said City of Kowloon should become part and parcel of Her Majesty's Colony of Hong Kong for all purposes during the continuance of the term of the Lease in the said Convention mentioned.

NOW, THEREFORE, Her Majesty is pleased, by and with the advice of Her Privy Council, to order, and it is hereby ordered as follows:

1. Article 4 of the Order of Her Majesty in Council of the 20th of October, 1898, is hereby revoked, without prejudice to anything lawfully done thereunder.

2. The City of Kowloon shall be and the same is hereby declared to be for the term of the Lease in the said Convention mentioned part and parcel of Her Majesty's Colony of Hong Kong in like manner and for all intents and purposes as if it had originally formed part of the said Colony.

3. The provisions of the said Order in Council of the 20th October, 1898, shall apply to the City of Kowloon in like manner as if the said City had by the said Order in Council been declared to be part and parcel of Her Majesty's Colony of Hong Kong.

And the Right Honourable Joseph Chamberlain, one of Her Majesty's Principal Secretaries of State, is to give the necessary directions herein accordingly.

A. W. FITZROY,

1899年12月27日英國頒佈的「樞密院令」

Royal Orders in Council, the 27<sup>th</sup> day of December, 1899

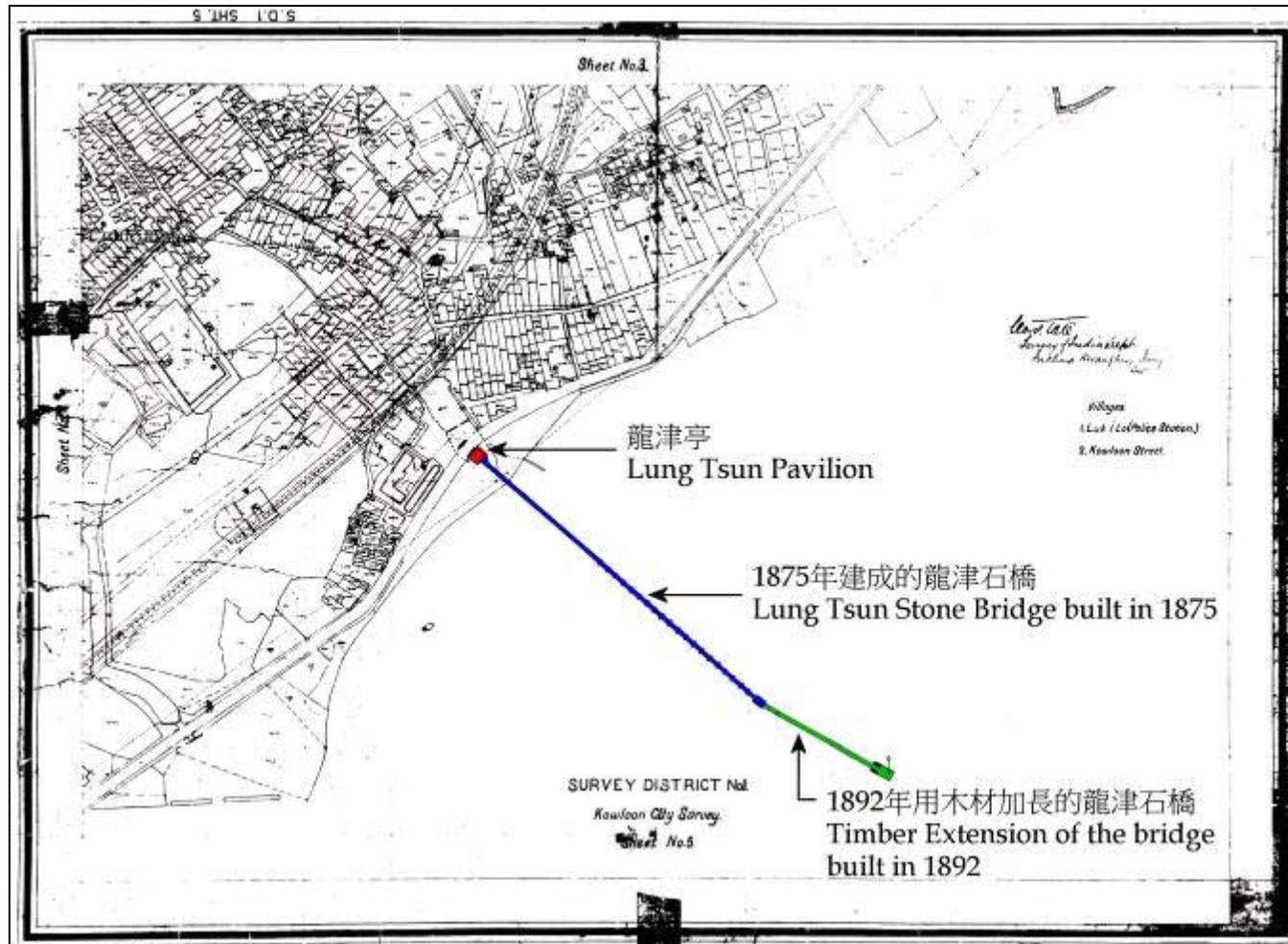
(Leung, 1995, pp.257-258.)

This is the place where the British flag is to be hoisted. The territory has been ceded by the Emperor of China to the Queen of Great Britain. I, being the Governor of Hongkong, have been deputed by my Government to receive the new territory, and I welcome you as friends and not as enemies. To-day is an important epoch in your lives for to-day you become British subjects. All the world over it is known that the ways of my country in ruling other people are excellent. We simply aim to make the people happy, and our country is respected by all the nations of the world. Our dominions spread over the four quarters of the world and millions upon millions of people own our protection. From this day of hoisting the flag you and your families and your property come under British protection. This territory to-day becomes part of Hongkong. If you, the Chinese, want to know how you will be treated, you can go to Kowloon and Hongkong and there see for yourselves. There you will find that all the Chinese are well protected and all their interests cared for. You may carry on your lawful occupations and your buying and selling unobstructed. Your ancestral temples and your temples for worshipping your gods will remain. Our Queen hopes that you will always enjoy prosperity and happiness, and I, as Governor, by command of Her Majesty's Government, hereby declare that your customs and usages will be respected. Village Courts will be established, and representatives will be selected from your gentry to assist in the management of public affairs, and while acting in accordance with the law you will be allowed perfect freedom. I do not say that existing regulations may not be altered, but the alterations will only be such as will meet with the approval of the law-abiding people. The taxes will be equal and the revenue will be collected justly. You need now have no fear of being squeezed by the officials. If exactions are made in excess of the just charges, the Government will dismiss the officials responsible. The taxes collected will be expended in maintaining order and in public improvements. I am going to make a road from this place to Shatin and thence to Kowloon so that you may easily transport your goods to Hongkong for sale. There will be no Customs charges or lekin<sup>1</sup> and you may freely bring back with you goods in exchange. You are now all British subjects, and can thus share in the benefits resulting from the prosperity of Hongkong. There you will find people who, starting with little capital have built up great businesses. You will all have the same chance of becoming prosperous. In all the villages we will establish schools and you will be protected in your rights; even the poorest people will be free from molestation. The laws that are made for your benefit must be obeyed, and all who break the law will be punished severely. All people, the gentry, the scholars and the common people must act honestly. From this time forth you are British subjects, and should you have any complaint to make the Governor will listen to it. No injustice will be allowed, nor any laxity. If you obey the law you need have no fear, and I hope you will all form one united community bound together by ties of love and respect. I pray God to afford you His protection and give you happiness.

港督卜力於 1899 年 4 月 17 日九龍城發表的演辭  
**The Speech of the Governor, Sir Henry Blake, delivered  
at Kowloon City on 17 April 1899**  
(Hase, 2008, pp.194-195)

## Maps

1. 1905 年九龍城測量圖  
Survey District No. 1 Kowloon City Survey Sheet No. 5, 1905
2. 1920 年代九龍城發展計劃圖  
Kowloon City Development Scheme Plan in 1920s
3. 1922-1935 年的測量圖 CXXXVII-NE-8  
Survey Sheet CXXXVII-NE-8, 1922-1935
4. 1926 年的啟德填海計劃平面圖  
Plan showing Kai Tak Reclamation, 1926
5. 1925 年九龍城飛機場平面圖  
Kowloon City Airfield, 1925
6. 1941 年啟德機場平面圖  
Kai Tak Airport, 1941
7. 1954 年啟德機場平面圖  
Kai Tak Airport, 1954
8. 1958-1974 年香港國際機場平面圖  
Hong Kong International Airport, 1958-1974
9. 1997 年香港國際機場平面圖  
Hong Kong International Airport, 1997



1905 年九龍城測量圖

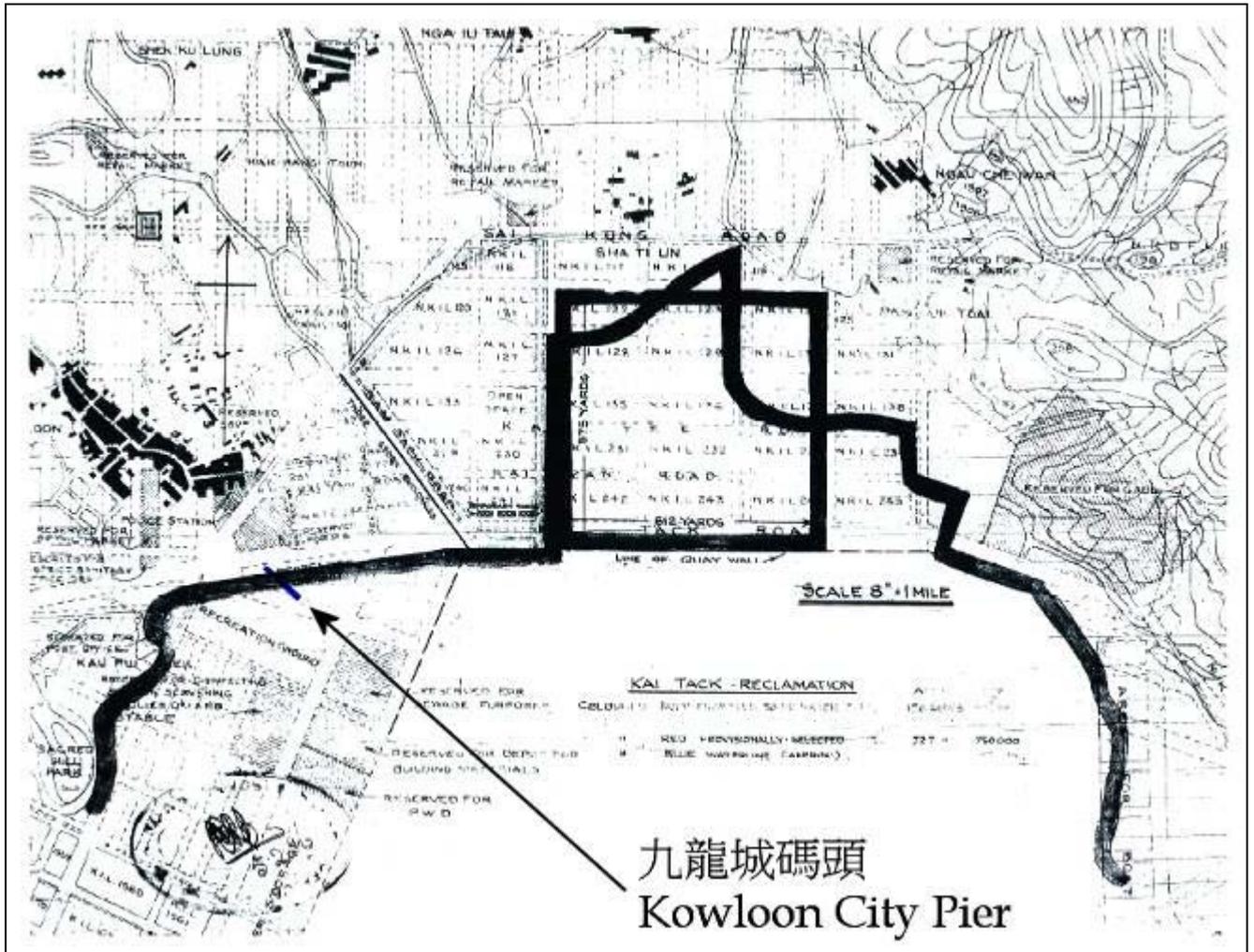
Survey District No. 1 Kowloon City Survey Sheet No. 5, 1905

(Survey and Mapping Office, 1905)



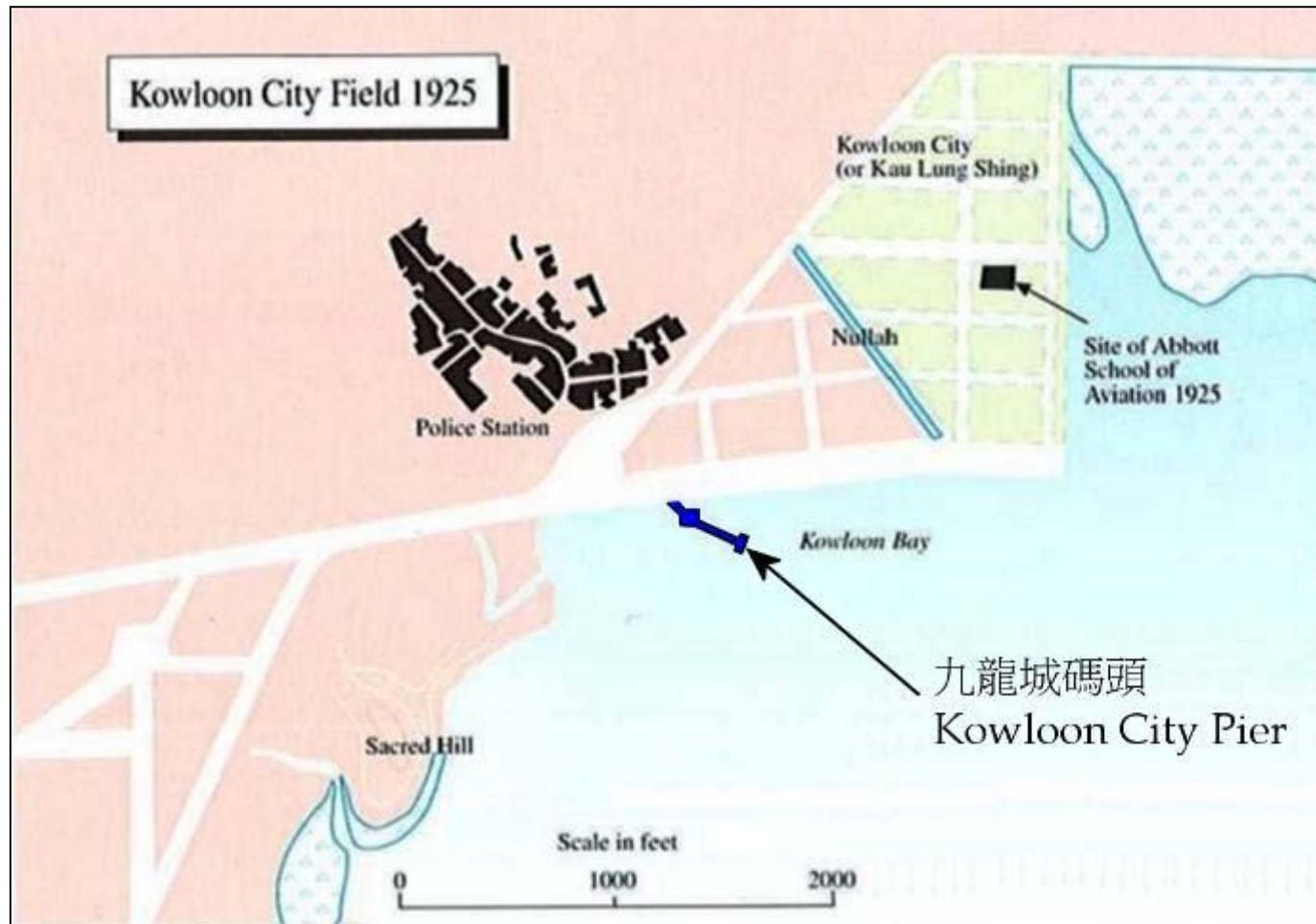


1922-1935 年的測量圖 CXXXVII-NE-8  
Survey Sheet CXXXVII-NE-8, 1922-1935  
(Survey and Mapping Office, 1922-1935)

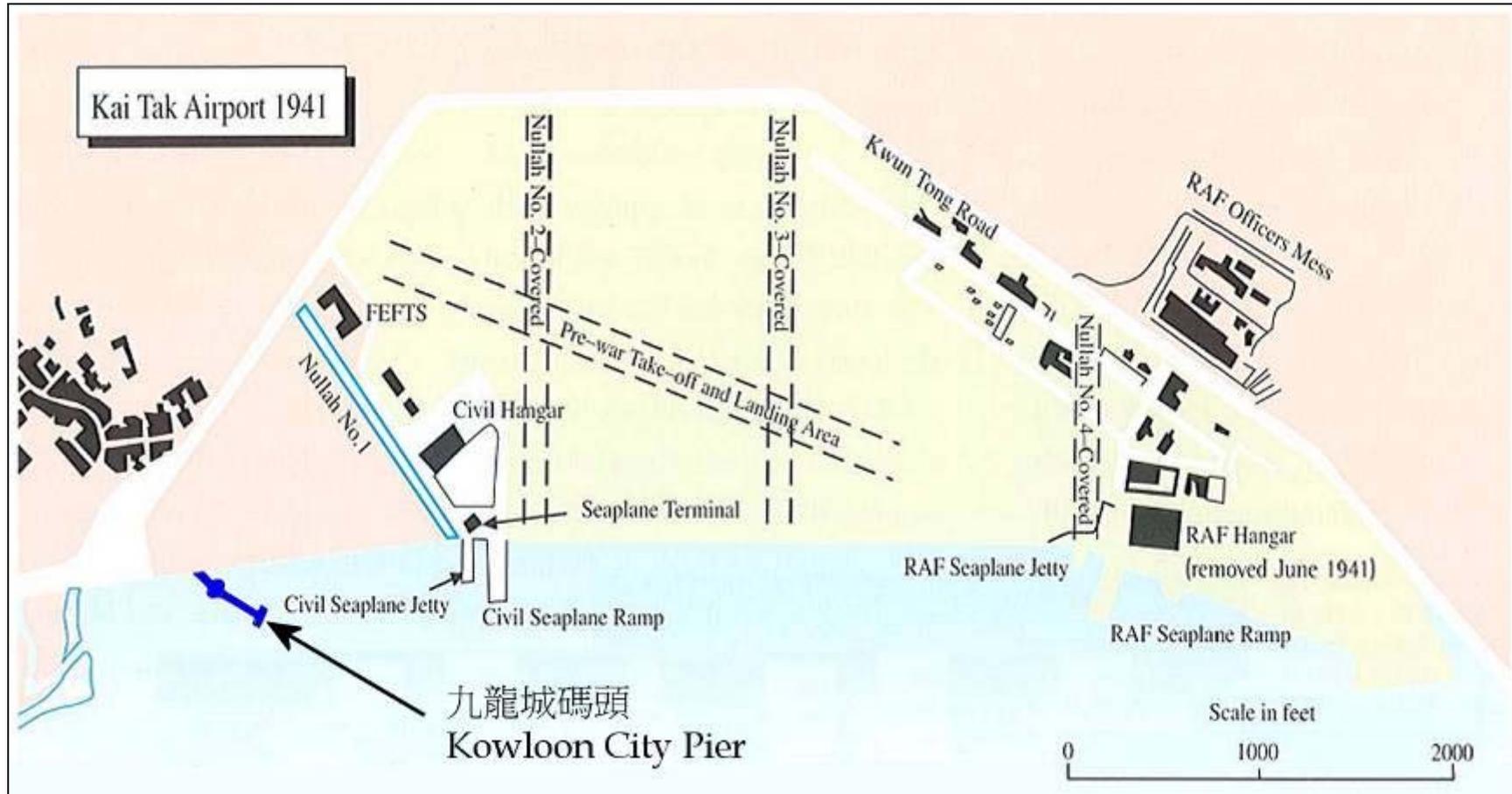


1926 年的啟德填海計劃平面圖

Plan showing the proposed Kai Tak Reclamation, 1926  
(CO-129-498 p460)



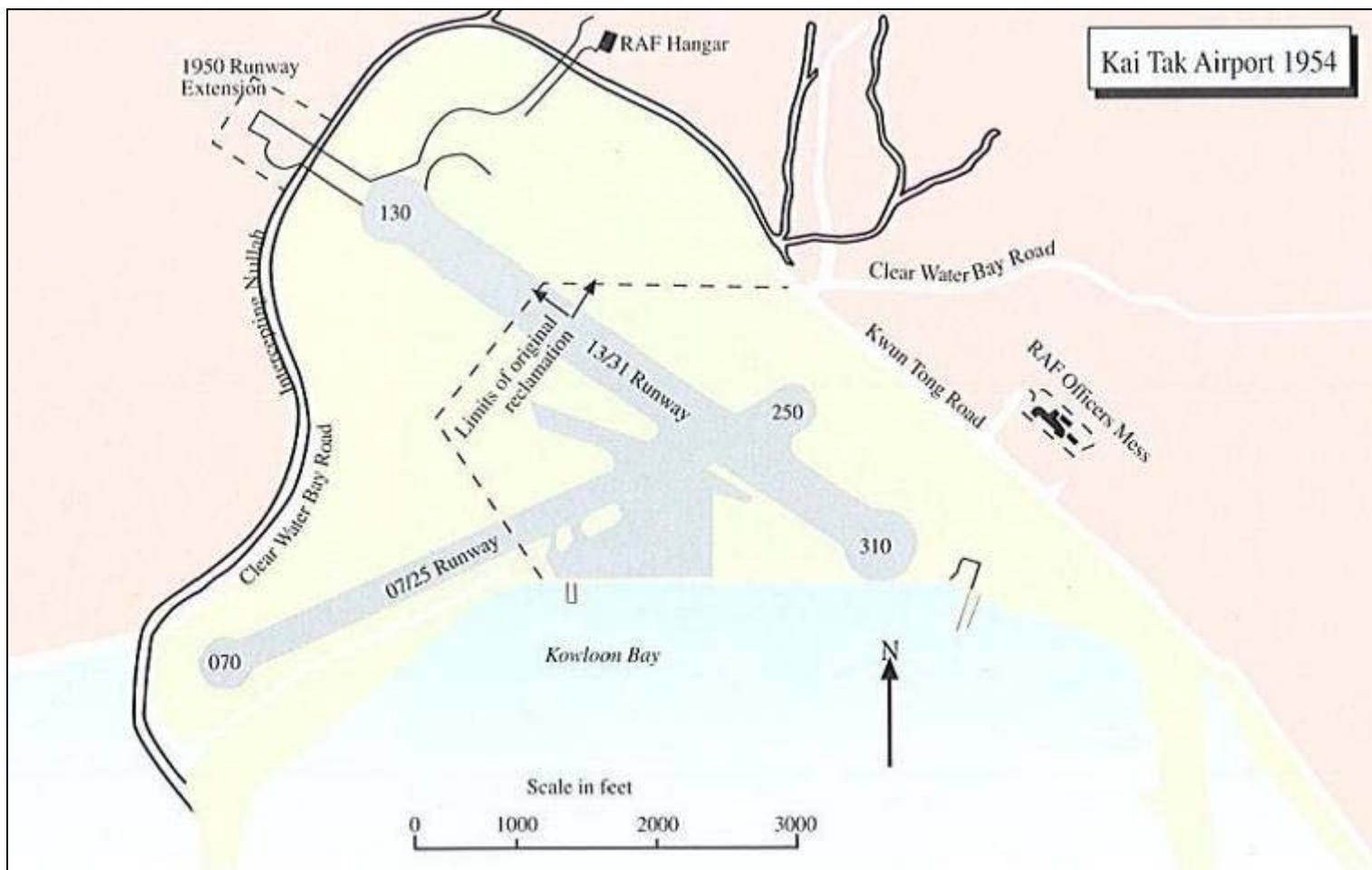
1925年九龍城飛機場平面圖  
Kowloon City Airfield, 1925  
("Wings over Hong Kong", 1998, p.190)



1941年啟德機場平面圖

Kai Tak Airport, 1941

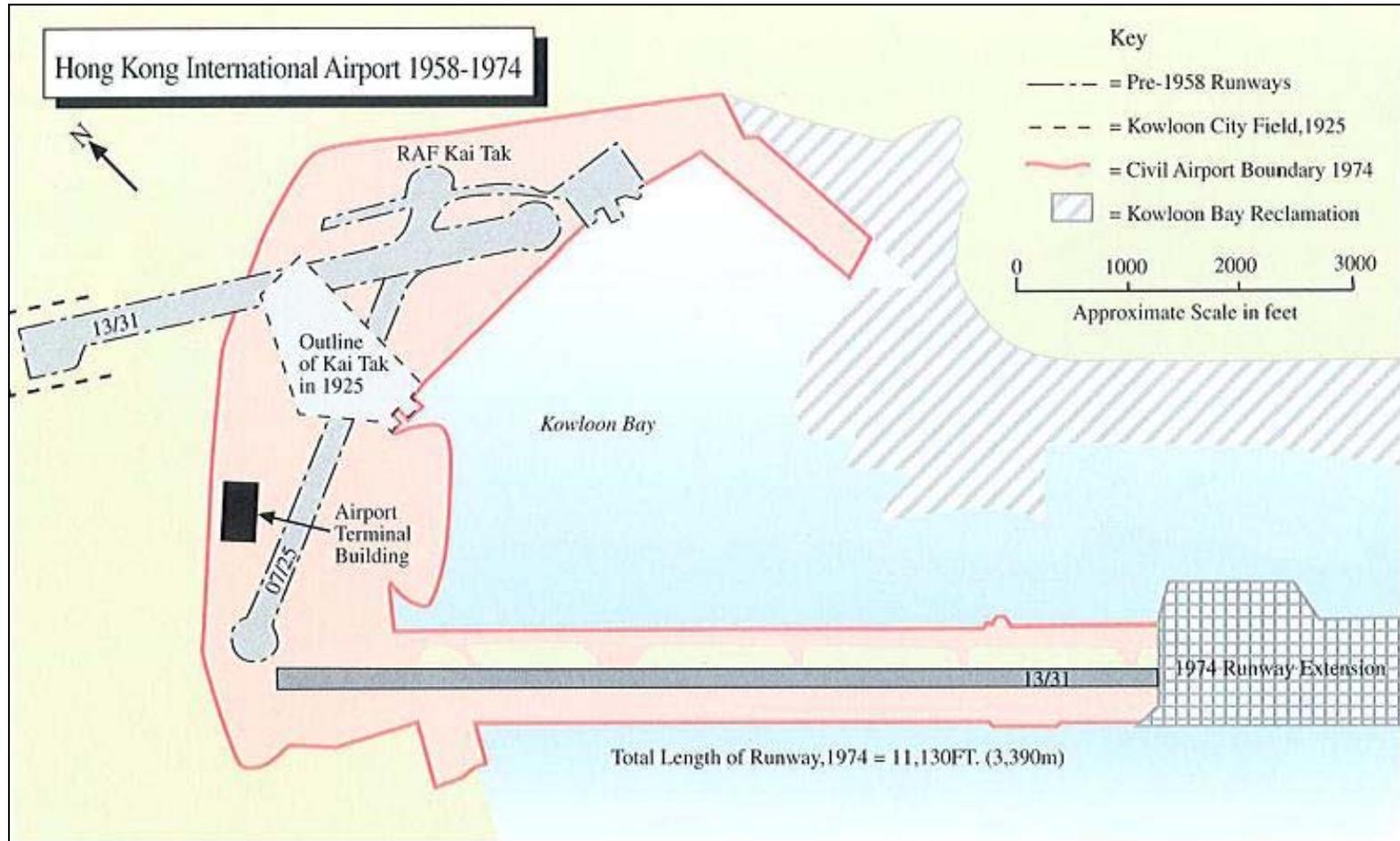
(*Wings over Hong Kong*, 1998, p.193)



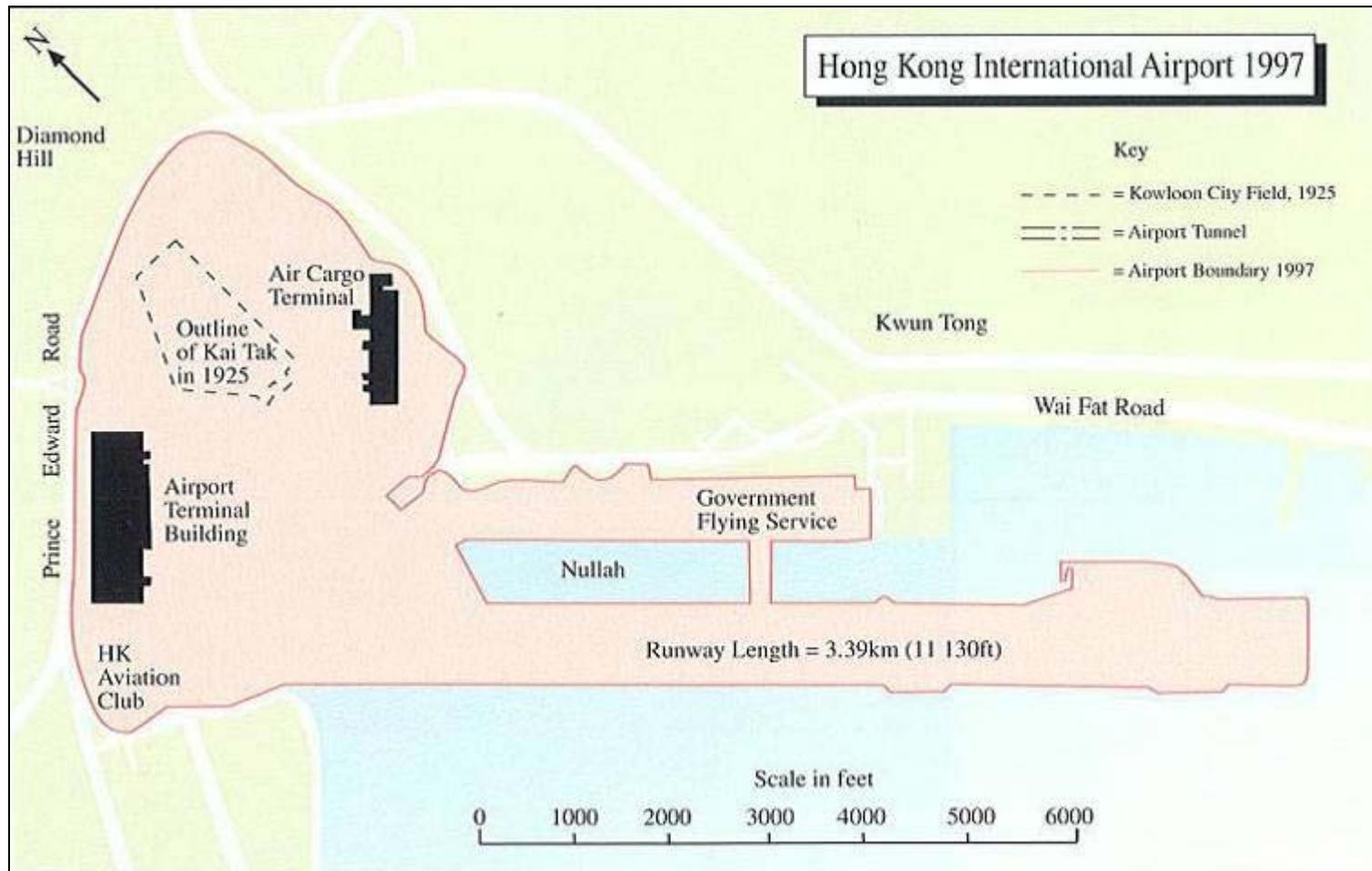
1954年啟德機場平面圖

Kai Tak Airport, 1954

(*Wings over Hong Kong*, 1998, p.198)



1958-1974 年香港國際機場平面圖  
**Hong Kong International Airport, 1958-1974**  
(*Wings over Hong Kong*, 1998, p.202)



1997 年香港國際機場平面圖  
**Hong Kong International Airport, 1997**  
(*Wings over Hong Kong*, 1998, p.202)

## Photographs

1. 龍津石橋及龍津亭(接官亭)  
Lung Tsun Stone Bridge and Lung Tsun Pavilion (Pavilion for Greeting Officials)
2. 從龍津石橋遠眺九龍寨城  
Kowloon Walled City viewing from the Lung Tsun Stone Bridge
3. 二十世紀初的龍津亭(接官亭)  
Lung Tsun Pavilion (Pavilion for Greeting Officials) in early 20<sup>th</sup> century
4. 龍津亭的石額  
Granite Lintel of Lung Tsun Pavilion
5. 龍津亭的石額，刻有「龍津」、「同治十三年甲戌仲夏」及「南海潘仕釗書」  
Granite Lintel of Lung Tsun Pavilion with Chinese characters
6. 龍津亭石額的近攝  
Close-up of Granite Lintel of Lung Tsun Pavilion
7. 1891 年位於九龍城的刑場，龍津石橋的側面可見於相中  
Kowloon City Execution Ground in 1891, side view of Lung Tsun Stone Bridge in the background
8. 二十世紀初九龍城的道路，遠方可見龍津石橋的側面  
A Road of Kowloon City in the early 20<sup>th</sup> century, side view of Lung Tsun Stone Bridge in the background
9. 1932 年的九龍城碼頭  
Kowloon City Pier, 1932



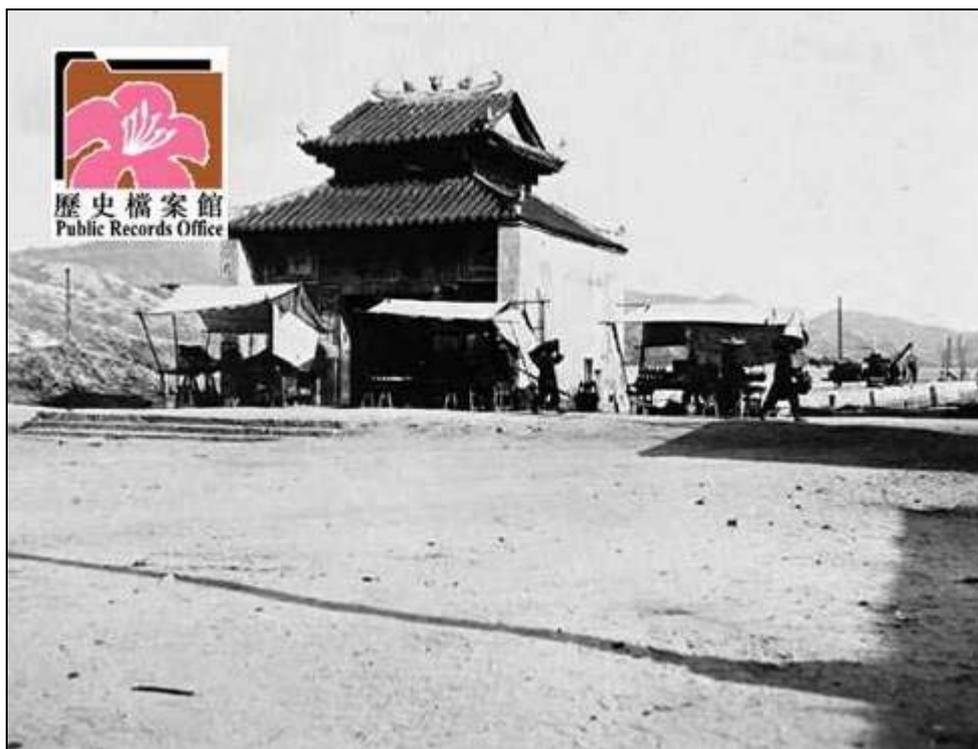
龍津石橋及龍津亭(接官亭)

**Lung Tsun Stone Bridge and Lung Tsun Pavilion (Pavilion for Greeting Officials)**  
(Public Records Office Photo # 03-03-065)



從龍津石橋遠眺九龍寨城

**Kowloon Walled City viewing from the Lung Tsun Stone Bridge**  
(Public Records Office Photo # 03-03-066)



二十世紀初的龍津亭(接官亭)

**Lung Tsun Pavilion (Pavilion for Greeting Officials) in early 20<sup>th</sup> century**  
(Public Records Office Photo # 03-04-073)



龍津亭的石額  
**Granite Lintel of Lung  
Tsun Pavilion**  
(Hong Kong Public  
Libraries Photo # 752199)



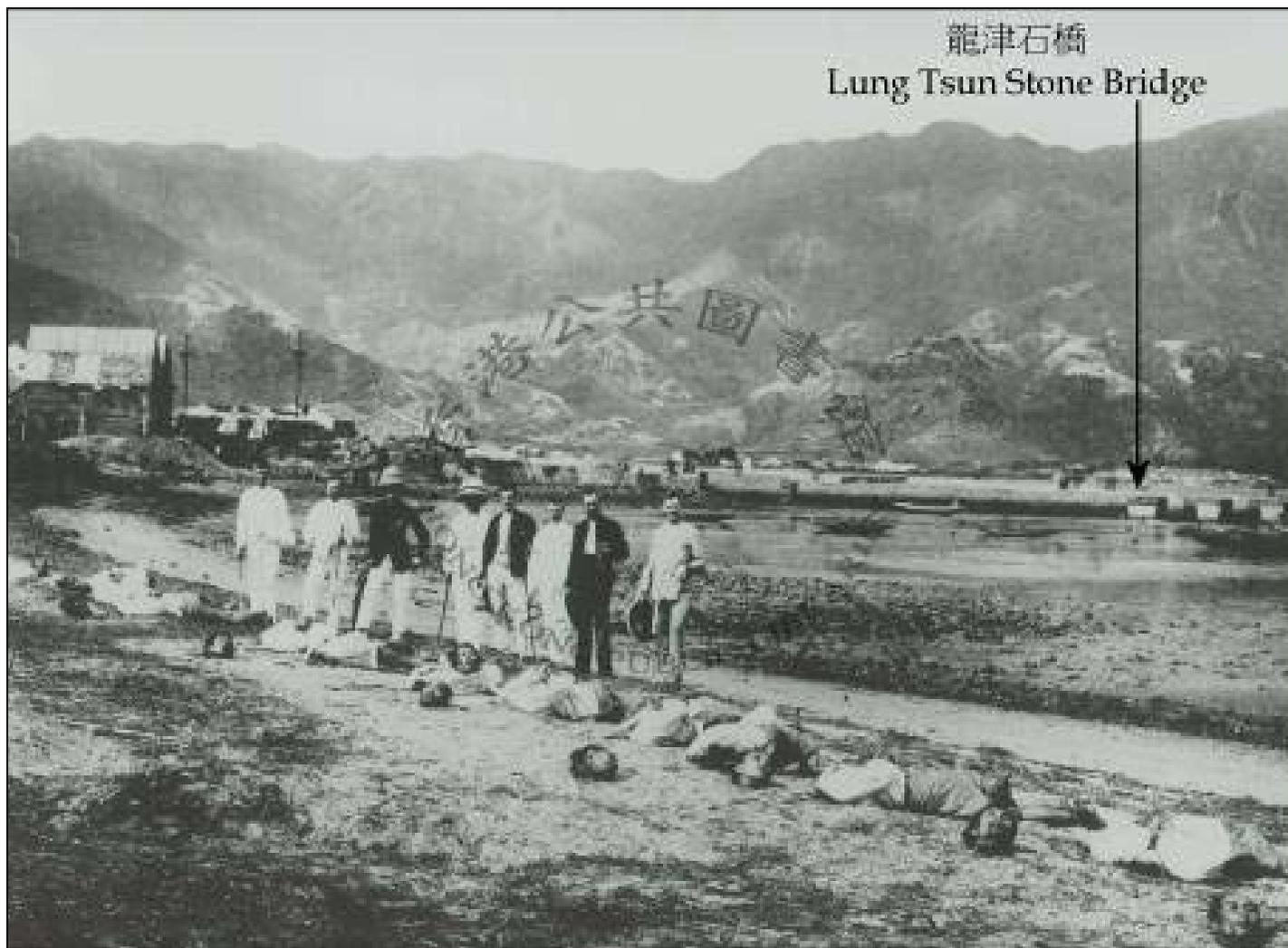
龍津亭的石額，刻有「龍津」、「同治十三年甲戌仲夏」及「南海潘仕釗書」

Granite Lintel of Lung Tsun Pavilion with Chinese characters



龍津亭石額的近攝

Close-up of Granite Lintel of Lung Tsun Pavilion



1891 年位於九龍城的刑場，龍津石橋的側面可見於相中  
**Kowloon City Execution Ground in 1891,**  
**side view of Lung Tsun Stone Bridge in the background**  
(Hong Kong Public Libraries Photo # 2084769)



二十世紀初九龍城的道路，遠方可見龍津石橋的側面

A Road of Kowloon City in the early 20<sup>th</sup> century, side view of Lung Tsum Stone Bridge in the background

(Hong Kong Public Libraries Photo # 752236)

龍津石橋的石礮  
Supporting Pillars of Lung Tsun Stone Bridge



1932 年的九龍城碼頭  
Kowloon City Pier, 1932.  
(Wu, & He, 2007, p.41)

## Hong Kong Government Records

1. "Statement of Expenditure (Actual and Estimated) for 1899 in connection with the New Territory", *Hong Kong Sessional Paper*, Hong Kong Government, 1899.
2. "Report on the New Territory for the year 1900", *Hong Kong Sessional Paper*, Hong Kong Government, 1901, p.20.
3. "Report of the Director of Public Works for the year 1910", *Hong Kong Administration Report*, Hong Kong Government, 1910, Appendix P, pp.43-44.
4. "Report of the Director of Public Works for the year 1916", *Hong Kong Administration Report*, Hong Kong Government, 1916, Appendix Q, p.46.
5. "Report of the Director of Public Works for the year 1921", *Hong Kong Administration Report*, Hong Kong Government, 1921, Appendix Q, p.41.
6. "Report of the Director of Public Works for the year 1922", *Hong Kong Administration Report*, Hong Kong Government, 1922, Appendix Q, p.52.
7. "Report of the Director of Public Works for the year 1923", *Hong Kong Administration Report*, Hong Kong Government, 1923, Appendix Q, p.65 & p.70.
8. "Report of the Director of Public Works for the year 1925", *Hong Kong Administration Report*, Hong Kong Government, 1925, Appendix Q, p.63.
9. "Report of the Director of Public Works for the year 1926", *Hong Kong Administration Report*, Hong Kong Government, 1926, Appendix Q, p.63.
10. "Report of the Director of Public Works for the year 1927", *Hong Kong Administration Report*, Hong Kong Government, 1927, Appendix Q, p.43.
11. "Report of the Director of Public Works for the year 1930", *Hong Kong Administration Report*, Hong Kong Government, 1930, Appendix Q, p.55.
12. "Report of the Director of Public Works for the year 1931", *Hong Kong Administration Report*, Hong Kong Government, 1931, Appendix Q, p.27.
13. Hong Kong Government Gazette 1934 (Supplement), no. S. 97
14. "Report of the Director of Public Works for the year 1934", *Hong Kong Administration Report*, Hong Kong Government, 1934, Appendix Q, p.51.
15. "Report of the Director of Public Works for the year 1936", *Hong Kong Administration Report*, Hong Kong Government, 1936, Appendix Q, p.54.
16. "Report of the Director of Public Works for the year 1937", *Hong Kong Administration Report*, Hong Kong Government, 1937, Appendix Q, p.47.

PUBLIC WORKS DEPARTMENT.	
<i>Expenditure to 31st July, 1899, charged to New Territory Account.</i>	
<i>Sub-heads.</i>	
	\$ c.
1. Buildings, .....	16,084.31
2. Landing Stage and Approach Road,.....	1,660.68
3. Preparation for Hoisting Flag,.....	424.46
4. Telegraphs and Telephones, .....	645.18
5. Roads,.....	15,558 70
6. Sanitation, .....	511.99
7. Surveys, .....	386.80
8. Wages, .....	136.92
9. Miscellaneous,.....	344.85
Total Actual Expenditure,.....	\$ 35,753.89
<i>Estimate of Expenditure for the remainder of the year.</i>	
	\$ c.
Completion of Táipó Police Station, .....	7,855.00
"    of P'ing Shán Police Station, .....	10,000.00
"    of Au T'au Police Station,.....	10,000.00
"    of Futian Police Station, .....	8,881.00
Expenditure on Táipó Road,.....	35,000.00
"    on Kowloon City Road, .....	18,000.00
Minor Miscellaneous Works, .....	2,000.00
Improvement of Official Quarters at Táipó in the winter, .....	1,000.00
Repair of Kowloon City Pier, <i>now dangerous</i> , .....	3,000.00
Expenses of Indian Survey Party, .....	5,000.00
Total Estimated Expenditure,.....	\$100,736.00
Total (Actual and Estimated), .....	\$136,489.89
HARBOUR DEPARTMENT.	
<i>Actual Expenditure to 31st July, 1899.</i>	
<i>Personal Emoluments.</i>	
Crew of <i>Wing Foo</i> , 15th to 30th April, .....	\$ 131.73
"    "    "    May, .....	247.00
"    "    "    June,.....	247.00
"    "    "    July,.....	247.00
	\$ 872.73
Inspector of Junks at Sham Shui-po at \$80 (10th to 31st July), .....	56.77
Total Personal Emoluments, .....	\$ 929.50
<i>Other Charges.</i>	
Hire of <i>Morning Star</i> April, .....	\$ 70 00
Hire of 2 Launches towing H.M.S. <i>Peacock</i> ,.....	25.00
Hire of 2 Cargo Boats, .....	20.00
3 Wire Stays, &c., .....	19.50
	64.50
Provisions sent to Táipó Hti, May, .....	15.79
Coal, Oil, Stores, &c. for <i>Wing Foo</i> , April, .....	\$ 136 55
"    "    "    May, .....	340.55
"    "    "    June, .....	332.21
"    "    "    July, .....	462.04
	1,271.35
Charter of <i>Wing Foo</i> 15th April to 31st December at \$600.00, .....	1,800.00
Total Other Charges, .....	\$ 3,221.64
Total Actual Expenditure,.....	\$ 4,151.14

"Statement of Expenditure (Actual and Estimated) for 1899 in connection with the New Territory", *Hong Kong Sessional Paper*, Hong Kong Government, 1899.

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## Appendix No. 4.

WORK DONE BY THE PUBLIC WORKS DEPARTMENT IN THE  
NEW TERRITORY, DURING THE YEAR 1900.

## BUILDINGS.

1. The large permanent two-storied Police Station containing 11 rooms at Au-tau near Ūn Long, containing accommodation for 6 Europeans, 21 Indians and Chinese, was completed, the total cost being \$13,286.83.

2. Plans and estimates were prepared and a contract let for a similar Station, but rather smaller, at Shataukok near to Startling Inlet at the Northern Boundary of the Territory. The building was finished and occupied before the end of the year. The total expenditure on it in the year was \$9,100.15.

3. Plans and estimates were also prepared and a contract let for a similar Station at Sai Kung on Shelter Bay. The masonry was well advanced before the end of the year, but the work has been slow owing partly to much sickness among the workmen. The expenditure in the year on this Station was \$2,628.05.

4. Ping Shán Police Station was finally completed at a total cost of \$13,514.15. It is similar to that at Au-tau.

5. The construction of a causeway to connect the hill on which the official quarters and the Police Station stand with an Island close to deep water at Táipó was undertaken under a contract amounting to \$4,887.70. The work when well advanced was twice partially destroyed by storms, and the contractor, who is unfortunately a man wanting in resources, has been disheartened and has made indifferent progress. The work will be finished this year. The expenditure in 1900 was \$2,030.72.

6. The matshed buildings at Táipó suffered severely in the somewhat severe gales which occurred in September and were almost destroyed in the typhoon of November. They have been restored and put in good order at a cost of \$1,051.

7. Telephone communication was extended to the temporary Police Station at San-r'in at a cost of \$944.04.

8. Shataukok Station was also connected with the Telephone system, the expenditure on it and other lines being \$852.

9. Táipó Road.—This road was completed down to sea level in the Shatin valley, a distance of 9 miles from Kowloon ferry, early in the year. The contractor who had constructed the road so far, and done good work, refused to go on with the next section except at very exorbitant increases in rates, nor was it found possible for some time to secure any contractor willing to take up the work at anything like reasonable rates.

However at last a man was found to take it on, but the progress during 1900 was not equal to that made in 1899. The 10th and 11th miles for a great part skirt the sea-shore of Shatin Inlet and some heavy bridging is necessary to carry the road over streams draining the southerly and easterly slopes of the Tái-mó-Shán range. The road is chiefly on embankment with the sea-face pitched with granite laid in mortar and pointed in cement. The masonry of the abutments and piers of the Tai Wai bridge, 3 spans of 30 feet clear, was completed in the year, and also that of the 30-foot bridge on the 10th mile. The eleventh mile is, for over 1,700 feet, carried on a causeway across a bay or arm of the sea so as to avoid a long detour. This was a difficult work, but has been successfully completed, and being shorter and more direct than a road hugging the sea-shore, will save in future maintenance. Except the fixing of the iron work, which had not arrived from England, on the large bridges, the road was practically completed and ready for traffic to 11¼ miles from Kowloon in December. The trace was carried on to Táipó, and a foot-path opened along it, the total distance to Táipó by this road turns out to be 18 miles.

10. The renewal of Kowloon City Pier was completed, the total cost being \$5,829.

11. The survey of the New Territory proceeded during the year under Mr. TATE, his Assistant Mr. NEWLAND carrying out the cadastral work. Progress has not been rapid and the work will evidently cost much more than was anticipated by this Government on the report of the Surveyor General of India. The total area cadastrally surveyed in the year was about 41,000 acres, and the total expenditure in the year was \$32,455.26.

It was expected that at least Mr. TATE'S work, namely the Trigonometrical Survey, would have been completed much sooner.

12. An iron and concrete bridge of two spans of 15 feet each was built on the Saikung Road just beyond Kowloon City.

13. The completed roads were well maintained during the year and were considerably used, that to Kowloon City proving that it met an urgent want of the constant stream of traffic pouring along it, and also on the cross road leading to Yaumati.

The Táipó Road has also come into use, persons visiting the New Territory, using it with carriages or jinrickshaws as far as Tai Wai on the 9th mile. Táipó is now easily within 4 hours' reach of Hongkong by road.

R. D. ORMSBY,  
Director of Public Works.

HONGKONG, 4th March, 1901.

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## KOWLOON.

Chatham Road (temporary route), - - - -	4
Salisbury Road, - - - - -	3
Approach road to Mortuary, Yaumati, - -	1
Shanghai Street, - - . - - - -	2
Fife Street, - - - - -	1
Argyle Street, - - - - -	2
Nelson Street, - - - - -	1
	—
	<u>14</u>
1910 Estimates, - - - - -	\$1,500.00
1910 Sup. Vote, - - - - -	600.00
	—
	\$2,100.00
1910 Expenditure, - - - - -	2,067.00

92. *Triangulation Survey of Colony.*—A description of the work accomplished will be found under paragraph 15 of this report. The designation of the Vote has been altered from "Permanent Marks for Traverse Survey Points in New Territories" to that given at the commencement of this paragraph.

1910 Estimates, .....\$5,000.00	Total Estimates,...	\$11,000.00
1910 Expenditure, ... 1,138.59	Expenditure to	
	31/12/10, .....	2,238.37

93. *Green Island and Kowloon City Piers.*—These piers were completed in September and October respectively. With the exception of the fenders, which are of Yacal, they are constructed entirely of reinforced concrete. The piles, which vary from 12" × 12" to 14" × 14" and from 15 $\frac{3}{4}$  to 37 $\frac{1}{2}$  feet in length, were made in Hongkong and were conveyed to the works when properly set and driven in the usual manner. The walings, bracings, deck beams and decking were all made *in situ* and, owing to the small range of tide and the infrequency of suitable tides, the work was a somewhat tedious one. It is however hoped that the additional trouble involved will be compensated for by the greater durability of the material employed compared with the timber hitherto used.

Green Island pier is L-shaped, is 90 feet in length and has an area of 1,350 square feet. It is provided with one set of steps and a pair of davits for hoisting a boat used in connection with the Gunpowder Dépôt.

Kowloon City pier is 149 feet long and has an area of 2,950 square feet, being provided with two sets of steps at the outer end, which is widened to accommodate them. To obviate unduly increasing the length of the pier, 21,608 cubic yards of mud were dredged in order to deepen the approaches, the seabed being comparatively level in the vicinity. The cost of the dredging was defrayed from the vote "Dredging Foreshores".

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The use of Kowloon City Pier as a regular place of call for two launches running a ferry service to that district was sanctioned subject to payment of \$20 and \$15 per month respectively.

The amount actually expended during the year was \$19,594.72, but a credit of \$12,306.18 was obtained by returning into Store the plant referred to in last year's report, thus reducing the amount to \$7,288.54.

1910 Estimates, .....	\$18,000.00	Total Estimates, ...	\$34,734.00
1910 Expenditure, ...	7,288.54	Expenditure to 31/12/10, .....	34,146.08

94. *Miscellaneous Works.*—The following are the principal items of expenditure under this heading, representing in some cases only a part of the cost of the works in consequence of their execution extending over more than one year:—

Western Market :—	\$	\$
Constructing fruit market on open space behind old Harbour Office (completed),	3,734	
Closing opening on first floor of new Western Market with iron gratings, - - -	291	
Enclosing verandah openings of do., - - -	148	
Converting North vestibule of do. into two stalls, - - - - -	119	
Providing two new counters in do., - - -	86	
Erecting cubicles in fitter's room in do., -	72	
Wire netting to windows and gates of do.,	144	
Erecting matshed in old Western Market,	415	
	-----	5,009
Royal Square :—		
Removing statue of H.M. King Edward VII to permanent site, - - - - -	459	
Laying out and turfing area adjoining King Edward's statue, - - - - -	1,171	
Arrangements in connection with the unveiling of statues ceremony, performed in 1909, - - - - -	534	
	-----	2,164
Imports and Exports Office :—		
Erecting timber building for office adjoining new Harbour Office, - - -	1,628	
Do. timber shed for waiting room, -	177	
Electric light fittings, fans, etc., - - -	227	
	-----	2,032
Low-lying area at junction of Nathan and Jordan Roads, Kowloon,—filling in, - - - - -		1,636
Rifle Range in King's Park, Kowloon,—Extensions: \$2,498—\$1,093 contributed from Volunteer funds, - - - - -		1,405
Slaughter Houses, Kennedy Town,—Addition to Shroffs' quarters, - - - - -		1,317
Sookunpoo Market—reconstructing drainage and relaying floors, etc., - - - - -		1,253

P.W.R. Kowloon.

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The details of the expenditure under this head are as follows:—

Labour for cleansing operations, .....	\$1,471.43
Repairs, .....	944.11
Tools for cleansing operations, .....	208.78
General incidental expenditure, .....	181.25
Total,.....	<u>\$5,805.57</u>

as against \$5,412.80 in the previous year.

**73. Gas Lighting.**—The total number of lamps in use at the end of the year was 304, an increase of 11 as compared with the previous year. The positions of the various additional lamps will be found in paragraph 119 of this Report.

**74. Electric Lighting.**—The number of electric lamps in use at the end of the year, all of which are incandescent, was 74, an increase of 6 as compared with the previous year. This is due to the extension of street lighting to the Taikoktsui and Hok Un Districts. Particulars of the positions of the additional lamps will be found in paragraph 119 of this Report. The lighting of Shamshuipo District, referred to in last year's Report under this heading, is now dealt with under a separate vote (see paragraph 87 of this Report).

**75. Maintenance of Praya Walls and Piers.**—The following is a statement of the principal items of expenditure under this vote:—

Kowloon City Pier—Renewing reinforced concrete beams and repairing masonry piers,.....	\$601.39
Tsim Sha Tsui Pier:—	
Renewing fenders, .....	\$384.42
Casting and fitting platform plates, ...	108.00
	492.42
Praya Walls, General—Minor repairs, .....	212.25

**76. Maintenance of Chinese Cemeteries.**—The work carried out under this heading has already been alluded to in paragraph 42 of this Report.

**77. Typhoon and Rainstorm Damages.**—The heavy rains of June caused numerous small landslides, whilst road-surfaces not treated with tar were badly scoured. The only damage of any magnitude was the bulging of a nullah-wall at Mongkoktsui, which necessitated the addition of some cement concrete counterforts, and the collapse of a portion of nullah-walling, supporting the roadway on the Yaumati-Kowloon City Road. Deposits of sand and debris had to be removed from the various trained nullahs.

**78. Maintenance of Water Works.**—A constant supply of water was maintained throughout the year, the quantity supplied

A considerable number of defective traps were renewed and a number of old disused drains were destroyed and filled in.

All metal work in connection with the drainage systems was inspected and, where necessary, repaired and tarred.

The details of the expenditure under this heading are as follows:—

Labour for cleansing operations, ... ..	\$5,338.87
Repairs, ... ..	1,534.07
Tools for cleansing operations, ... ..	298.07
General incidental expenditure, ... ..	...
Total... ..	<u>\$7,171.01</u>

as against \$6,504.47 in the previous year.

68. *Gas Lighting*.—The total number of lamps in use at the end of the year was 353, an increase of 5 over the previous year.

69. *Electric Lighting*.—The number of electric lamps in use at the end of the year, all of which are incandescent, was 142, an increase of 2 as compared with the previous year.

70. *Maintenance of Praya Walls and Piers*.—The principal items executed under this heading were general repairs to Kowloon City Pier, Tsim Sha Tsui Pier and Shamshuipo Ferry Pier at the cost of \$1,046.97, \$704.93 and \$628.44, respectively.

71. *Maintenance of Chinese Cemeteries*.—The work carried out under this heading has been alluded to already in paragraphs 20 and 36 of this Report.

72. *Maintenance of Recreation Grounds*.—The use of departmental labour for keeping these grounds in good order was continued during the year.

73. *Typhoon and Rainstorm Damages*.—The heavy rains already referred to in paragraph 29 of this Report caused numerous small landslips throughout the district.

The walling at Ho Mun Tin referred to in paragraph 72 of last year's Report was completed.

The surfaces of those roads treated with asphaltum suffered only to a trifling extent, but practically the whole of the ordinary macadamized roads had to be repaired, owing to scour.

74. *Maintenance of Water Works*.—A constant supply of water was maintained throughout the year, the total quantity supplied being 617.73 million gallons giving an average daily consumption of 1.69 million gallons or 12.9 gallons per head per day, taking an estimated population of 130,420 (1921 Census figures).

A new Contract for 3 years to commence from the 1st of July, 1922, was entered into with the Hongkong and China Gas Co., for lighting Public Highways.

68. Electric Lighting.—The number of electric lamps in use at the end of the year—all of which are incandescent—was 173, an increase of 31 as compared with the previous year.

69. Maintenance of Praya Walls and Piers.—The principal items executed under this heading were general repairs to Tsim Sha Tsui Pier and Shamshui-po Pier at a cost of \$1,634.62 and \$1,251.33 respectively. Considerable repairs were also executed to Hunghom Pier, Kowloon City Pier, Shantung Street Pier, Waterloo Road Pier and to a portion of the sea-wall to the South of K.M.L. 32, Tai Kok Tsui.

70. Maintenance of Chinese Cemeteries.—The work carried out under this heading has already been alluded to in paragraph 45 of this Report.

71. Maintenance of Recreation Grounds.—The use of departmental labour for keeping these grounds in good order was continued during the year.

72. Typhoon and Rainstorm Damages.—The heavy rains already referred to in paragraph 28 of this Report caused numerous small landslips throughout the district.

The surfaces of those roads treated with asphaltum suffered only to a trifling extent, but practically the whole of the ordinary macadamized roads had to be repaired owing to scour.

73. Maintenance of Water Works.—A constant supply of water was maintained up to the 8th June when, owing to the failure of the usual Spring rains, it was found necessary to curtail the supply in the Chinese Districts to three hours a day and by 17th June general restriction in all Districts had to be adopted.

Restrictions were withdrawn on 23rd June following two days heavy rains but general restriction had again to be resorted to on 7th July till the 8th September when it was again found possible to give a full supply.

The total quantity supplied was 563.73 million gallons giving an average daily consumption of 1.54 million gallons or 11.4 gallons per head per day, taking an estimated population of 135,460.

Details are given in Annexe H.

The quantity of water stored in the impounding reservoir on the 1st January amounted to 269.76 million gallons and it reached a minimum on the 20th June when it amounted to 29.52 million gallons. The reservoir was at or above its permanent overflow level from 6th September to 15th November. The quantity of water remaining in the reservoir at the end of the year amounted to 291.34 million gallons.

**74. Electric Lighting.**—The number of electric lamps in use at the end of the year, all of which are incandescent, was 147 a decrease of 26 lamps as compared with the previous year, this being due to the installation of higher candle power lamps.

**75. Extension of Lighting.**—169 lamps were erected during the year in Kowloon and Shamshuipo Districts—46 electric and 123 gas.

Improved iron standards were erected in Chatham Road from Austin Road to Gascoigne Road, the 16 c.p. lamps being replaced by 100 c.p. lamps.

The central standards in Gascoigne Road and the side standards in Chatham and Coronation Roads were removed, and 400 c.p. centrally suspended lights installed, these being supported by new iron side standards.

1923 Estimates,.....	\$1,500.00
1923 Sup. Vote,.....	5,500.00
	\$7,000.00
1923 Expenditure, .....	\$6,958.63

**76. Maintenance of Praya Walls and Piers.**—The principal items executed under this heading were general repairs to Tsimshatsni Pier and Kowloon City Pier at a cost of \$950.07 and \$585.93 respectively. Repairs were also executed to Hunghom Pier, Waterloo Road Pier, Shan Tung Street Pier, Devils Peak and Shamshuipo Piers.

**77. Maintenance of Chinese Cemeteries.**—The work carried out under this heading has already been alluded to in paragraph 38 of this Report.

1923 Estimates, .....	\$1,000.00
1923 Expenditure, .....	\$ 961.34

**78. Maintenance of Recreation Grounds.**—The use of departmental labour for keeping these grounds in good order was continued during the year.

**79. Typhoon and Rainstorm Damages.**—The buildings erected by the Architectural Department during the past 10 or 15 years suffered little or no damage in the typhoon of August last and subsequent rains in October, but practically all the older buildings suffered to some extent, those principally affected being :—

*Green Island Powder Depôt* :—Pier and sea-wall suffered very badly necessitating the reconstruction of practically the whole of the pier and a large portion of the sea-wall.

The decking of Kowloon City Pier was badly damaged, three bays being completely carried away by the heavy seas. A new reinforced concrete and wood-decking has since been constructed.

The Taipo-Shun Wan Road was also damaged to some extent.

Several large landslips occurred throughout the district, especially on the Coastal and Taipo Roads.

The aerial lines on Lan Tau Island and in the New Territories were damaged during the typhoon of 18th August, but were expeditiously repaired.

**90. Maintenance of Water Works, Lai-chikok.**—The total quantity of water supplied during the year amounted to 161.28 million gallons which is an increase of 3.83 million gallons over 1922, or an average consumption of 441,000 gallons per day.

Details of consumption are given in Annexe J.

There were 20 meters in use at the close of the year.

**91. Maintenance of Water Works, Fanling.**—The total quantity of water supplied during the year amounted to 5.75 million gallons, or an average consumption of 15,000 gallons per day.

There were 14 meters in use at the close of the year.

**92. Maintenance of Water Works, Taipo.**—The new supply was turned on early in the year, but no figures are available as no meters were in use.

House services of Government Quarters were connected to the new main, and street fountains were provided at Taipo Market and Taipo Old Market.

**93. Water Accounts.**—Meters were examined and repaired in 10 instances.

The expenditure under the vote was as follows :—

New Meters (difference in value between issues and receipts), ... ..	\$550.45
New Installations, ... ..	2.02
Repairs to Meters (Labour), ... ..	16.61
Repairs to Meters (Materials), ... ..	...
Meter Boxes, etc., ... ..	...

Total, ... .. \$ 569.08

On account of the high tenders received for the Annual Maintenance Contract of 1925, the whole of the maintenance of sewers, nullahs, etc., was carried out departmentally.

On Roads and Streets where bridges are provided over nullahs traversing them, parapet-walls were erected on both sides of the bridges:—Waterloo Road nullah, at Nathan Road, Portland Street, Shanghai Street, Reclamation Street, and Canton Road. Argyle Street nullah, at Coronation Road and Portland Street. Nelson Street nullah, at Portland Street, Shanghai Street, Reclamation Street, and Canton Road.

Repairs were made to the sewers &c. at the undermentioned localities:—In Joss Street and Anchor Street; in front of 7, Ashley Road, Tsim Sha Tsui; in front of 55, Woosung Street; at the junction of Waterloo Road and Dundas Street; in front of 55 and 57, Nathan Road; in Reclamation Street at its junction with Hamilton Street; at the corner of Tai Wan Road and Kowloon City Road; and in Shantung Street at its junction with Thistle Street.

A considerable number of defective traps was renewed, and a number of old disused drains destroyed and filled in.

All metal work in connection with the drainage systems was inspected, and, where found necessary, repaired and tarred.

The details of expenditure under this heading were as follows:—

Labour for cleansing operations .....	\$4,455.14
Repairs .....	2,215.07
Tools for cleansing operations .....	353.93
General incidental expenditure .....	549.81
	\$7,573.95

as against \$4,215.87 in the previous year.

**75. Gas Lighting.**—The total number of lamps in use at the end of the year was 516, an increase of 19 over the previous year.

**76. Electric Lighting.**—The number of electric lamps in use at the end of the year, all of which are incandescent, was 234, an increase of 53 as compared with the previous year.

**77. Extension of Lighting.**—69 lamps were installed during the year in Kowloon and Shamshuipo Districts, 50 being electric and 19 Gas.

**78. Maintenance of Praya Walls and Piers.**—The principal items executed under this heading were general repairs to Tsim Sha Tsui Pier and Public Square Street Pier at a cost of \$2,076.40 and \$636.19 respectively. Repairs were also executed to Kowloon City Pier, Waterloo Road Pier and Shantung Street Pier.

**75. Maintenance of Sewers, Nullahs, &c.**—The sewers, stormwater drains, and trained nullahs were cleansed, repaired and maintained in good condition, the open channels and nullahs being cleansed by the Sanitary Department. The automatic flushing tanks were kept working continuously and sand deposits as they accumulated were removed.

Repairs were made to the sewers etc. at the undermentioned localities:—Latrine at the junction of Canton and Jordan Roads; at North-West and South-West of K.I.L. 1011; at the junction of Canton Road and Dundas Street; at the junction of Chang Sha and Shanghai Streets; at the junction of Reclamation, Dundas and Hamilton Streets; at Waterloo Road Nullah; between K.M.L.'s 67 and 72, To Kwa Wan; in front of No. 39, Cooke Street, Hung Hom; and at Kun Chung Market off Jordan Road.

Many defective traps, gullies, gratings etc. were renewed, and a number of old disused drains destroyed and filled in.

All metal work in connection with the drainage systems was inspected, and, where found necessary, repaired and tarred.

The details of expenditure under this heading were as follows:—

Labour for cleansing operations .....	\$ 3,457.73
Repairs .....	575.36
Tools for cleansing operations .....	390.63
General incidental expenditure .....	500.75
	\$ 4,924.47

as against \$7,573.95 in the previous year.

**76. Gas Lighting.**—The total number of lamps in use at the end of the year was 525, an increase of 9 over the previous year.

**77. Electric Lighting.**—The number of electric lamps in use at the end of the year, all of which are incandescent, was 288, an increase of 54 as compared with the previous year.

**78. Extension of Lighting.**—63 lamps were installed during the year in Kowloon and Shamshuipo Districts, 54 being electric and 9 gas.

**79. Maintenance of Praya Walls and Piers.**—The principal items executed under this heading were general repairs to Kowloon City Pier, Tsim Sha Tsui Public Pier and Mong Kok Pier at a cost of \$1,710.00, \$1,016.71 and \$535.90 respectively. Repairs were also executed to Yaumati Ferry Pier, Hunghom Ferry Pier, and Yaumati Fire Float Pier.

## — Q 43 — P.W.R. New Kowloon.

The nullah on the East side of the Kau Lung Tong Estate and the stream connection North of the hill area also suffered damage; the clearing of silt and repairs to them were completed by the end of November.

Preventive measure in the form of a bund was undertaken along the Railway Reserve to the West of the Kau Lung Tong Estate to obviate the possibility of damage arising to the Railway track as well as on the East side of the Eastern nullah to avoid the washing of silt on to the nullah.

Estimates, .....	\$30,000.00
Sup. Vote, .....	7,500.00
	\$37,500.00
Expenditure, .....	\$27,204.32

**85. Maintenance of Waterworks at Lai Chi Kok.**—The total quantity of water supplied during the year was 165.22 million gallons which is an increase of 107.98 million gallons over the figures for 1926 or an average consumption of 452,000 gallons per day. The details of consumption are given in Annexe J.

There were 21 meters in use at the close of the year, of which 3 were at Cheung Sha Wan.

Estimates, .....	\$5,000.00
Expenditure, .....	\$4,831.10

**86. Water Account (Meters, etc.).**—The number of meters examined and repaired during the year was 136.

Estimates, .....	\$600.00
Expenditure, .....	\$564.10

**87. Maintenance of Praya Wall and Piers.**—The Praya Wall and Piers were maintained in a good condition. The principal items were general repairs to Sham Shui Po Ferry Pier, Kowloon City Pier and Tung Chow Street Pier at a cost of \$1,146.08, \$399.79 and \$299.59 respectively.

Estimates, .....	\$ 100.00
Sup. Vote, .....	2,500.00
	\$2,600.00
Expenditure, .....	\$1,910.30

**88. Maintenance of Chinese Cemeteries.**—No work was carried out under this heading during the year.

Estimates, .....	\$100.00
Expenditure, .....	Nil.

## — Q 55 — P.W.R. New Kowloon.

**89. Maintenance of Water Works, Lai Chi Kok.**—The total quantity of water supplied during the year was 100.04 million gallons which is an increase of 7.13 million gallons over 1929 or an average consumption of 274,000 gallons per day.

Details of consumption are given in Annexe J.

There were 22 meters in use at the close of the year.

Estimates .....	\$4,000.00
Expenditure .....	\$2,792.46

**90. Water Account (Meters &c.).**—The number of meters examined and repaired during the year was 31.

Estimates .....	\$1,200.00
Expenditure .....	\$ 974.44

**91. Maintenance of Praya Walls and Piers.**—The Praya Walls and Piers were maintained in a satisfactory condition. Repairs were carried out during the year as follows:—

Fixing of fenders and repairs to quay walls of Water Boat Dock, Laichikok; the replacement of eight decayed timber piles by 8 reinforced concrete piles and repairs to sea wall at Kowloon City Pier, and repairs to landing steps of Shamshuipo Pier.

Kowloon City ferry pier was rewired and navigation light installed.

Estimates, .....	\$10,000.00
Expenditure, .....	\$ 5,408.92

**92. Maintenance of Chinese Cemeteries.**—Work carried out under this head has already been alluded to in paragraph 34 of this Report.

Estimates .....	\$1,000.00
Expenditure .....	\$ 797.04

**93. Bathing Beach, Lai Chi Kok.**—The provision of this facility was referred to in paragraph 69 of last year's Report.

The attendance for this beach showed a very slight improvement over last year, although there was a marked falling off during the last three months of the season.

Estimates .....	\$3,000.00
Expenditure .....	\$2,388.62

— Q 27 — P.W.R. New Kowloon.

**170. Water Account (Meters &c.).** There were twenty-six meters in use at the close of the year as compared with twenty-two in 1930. The number of meters examined and repaired during the year was thirty-two.

Expenditure ..... \$2,094.54

**171. Maintenance of Praya Walls and Piers.** The Praya Walls and Piers were maintained in a satisfactory condition. This necessitated repairs to the following works:—Kowloon City pier; Pitched Slope at Lai Chi Kok Water Boat Dock, and minor repairs to Shamshuipo pier and Dust Boat Sattion.

Expenditure ..... \$1,725.04

**172. Maintenance of Chinese Cemeteries.** Work carried out under this head is referred to in paragraph 384 of this Report.

Expenditure ..... \$1,507.63

#### NEW TERRITORIES.

**173. Maintenance of Buildings.**—Government buildings were kept in a good state of repair. Painting, colourwashing and repairs in accordance with the recurring programme were carried out to five buildings. Major repairs included the following:—Repairs to white ant damage at following places—Tai Po Police Station, Sheung Shui Police Station, Government Quarters Tai Po, Tai Po Island Quarters and Police Bungalow, Repairs to barbed wire entanglements at Cheung Chau, Lok Ma Chau and Sheung Shui Police Stations.

Expenditure ..... \$12,548.53

**174. Improvements to Buildings.** Only minor improvements to a few buildings were carried out under this head.

**175. Maintenance of Lighthouses.** All Lighthouses were maintained in a good state of repair, although painting and colourwashing in accordance with the recurring programme were not carried out.

Expenditure ..... \$1,101.77

**176. Roads and Bridges (Maintenance and Improvements).** The surfaces of approximately eighty miles of roads were generally maintained in a satisfactory condition. The strengthening and improvement of the road surfaces was continued, an additional layer of 4" macadam being laid over the present surface and tarpainted between the 10th and 14th miles on the Coastal Road.

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## PUBLIC WORKS DEPARTMENT.

**No. S. 97.**—It is hereby notified that sealed tenders in triplicate, which should be clearly marked "Tender for Raising Level of Causeway to Kowloon City Pier", will be received at the Colonial Secretary's Office until Noon of Tuesday, the 20th day of March, 1934. The contract comprises the dumping of pell-mell rubble to form a causeway to Kowloon City Pier and the surfacing of same with cement concrete and any other contingent works.

For form of tender, specification and further particulars apply at this Office.

The Government does not bind itself to accept the lowest or any tender.

R. M. HENDERSON,  
*Director of Public Works.*

7th March, 1934.

## PUBLIC WORKS DEPARTMENT.

**No. S. 98.**—It is hereby notified that sealed tenders in triplicate which should be clearly marked "Tender for Furniture Store and Workshop, etc.," will be received at the Colonial Secretary's Office until Noon of Monday, the 26th March, 1934. The work consists of the erection of a two storied Workshop, Store, and Latrine at Hung Hom.

No work will be permitted on Sundays.

For form of tender, specification and further particulars apply at this Office.

The Government does not bind itself to accept the lowest or any tender.

R. M. HENDERSON,  
*Director of Public Works.*

7th March, 1934.

## PUBLIC WORKS DEPARTMENT.

**No. S. 99.**—It is hereby notified that sealed tenders in triplicate, which should be clearly marked "Tender for Improving bends at 5 $\frac{1}{4}$ , 6 $\frac{3}{4}$  and 7 $\frac{1}{2}$  Miles, Tai Po Road", will be received at the Colonial Secretary's Office until Noon of Tuesday, the 27th day of March, 1934, for the improvement of bends on the Tai Po Road, New Territory, near 5 $\frac{1}{4}$ , 6 $\frac{3}{4}$  and 7 $\frac{1}{2}$  Miles respectively to their proposed new alignments together with all contingent works.

No work will be permitted on Sundays.

For form of tender, specification and further particulars apply at this Office.

The Government does not bind itself to accept the lowest or any tender.

R. M. HENDERSON,  
*Director of Public Works.*

9th March, 1934.

## — Q 51 —

## WATER WORKS.

182. *Back Lane Service Pipes*:—Thirty new back lane service pipes were laid which involved the installation of 200 feet of 3", 3,215 feet of 2" and 191 feet of 1½" wrought iron piping. 201 houses were connected to these and existing subsidiary mains during the year.

Expenditure ..... \$9,979.61

183. *Distribution Mains*:—The following work was carried out under this head:—740 feet of 18" cast iron main were removed from Taipo Road near the junction with Castle Peak Road, 637 feet of 12" cast iron main were laid in Cheung Sha Wan Road westwards to Yen Chow Street, 830 feet of 10" cast iron main were laid in Castle Peak Road from 4½ Mile to Lai Chi Kok Water Boat Dock, 456 feet of 10" cast iron main were laid in Castle Peak Road near 3½ Mile, 1,490 feet of 8" cast iron main were removed from the old Castle Peak Road from Lai Chi Kok Stream, eastwards, 485 feet of 8" cast iron main were laid in Grampian Road, 1,490 feet of 6" cast iron main were laid in Boundary Street from Grampian Road to La Salle College, 570 feet of 6" cast iron main were laid in Boundary street from Taipo Road to Fa Yuen Street, 530 feet of 6" cast iron main were laid in Carpenter Road between Sa Po Road, and South Wall Road, 353 feet of 6" cast iron main were laid in Grampian Road, 260 feet of 6" cast iron main were laid in Nga Tsin Wai Road and 550 feet of 6" cast iron main were laid in Wong Chuk Street from Cheung Sha Wan Road to Ki Lung Street.

Expenditure ..... \$14,275.02

## PORT WORKS.

184. *Raising Level of Causeway to Kowloon City Pier*:—A contract for this work was let to Messrs. Lam Woo & Co. for \$2,440.00. Progress was slow but all construction was completed on the 5th December.

Expenditure ..... \$1,236.34

185. *Formation of Dumping ground at Kun Tong*:—This work was referred to in paragraph 233 of last year's Report. A contract was let to Messrs. Kwan On who commenced work on the 28th February and the whole of the works embraced in the contract were completed on the 29th November. The total quantity of rubble deposited to form the protective mound was 206,733 cubic yards. Nine 12-foot lengths of walling and one monument were built at intervals to mark the position and lines of the mound.

Expenditure ..... \$399,340.41

Expenditure to 31st Dec., 1934.....\$403,645.04

## — Q 54 —

201. *Improvement to Tytam Road*:—The entrance to Tytam Road was widened, and an area for parking was formed. The cost was covered by payment into a separate account.

202. *Road from Kennedy Road to Military Hospital, Bowen Road*:—The work comprises an 18 feet wide road leading from Kennedy Road to the Military Hospital connecting with Bowen Road on a level at one point and also passing on a bridge over Bowen Road to give direct access to the Hospital Compound. The cost was covered by payment into a separate account.

203. *Dredging for Harbour Improvements*:—This work was referred to in paragraph 194 of last year's Report. 233,800 cubic yards of material were dredged and deposited at the Kun Tong Dumping Ground. The cost was covered by payment into a separate account.

204. *Ho Mun Tin Hill Sewerage Scheme*:—The construction of a 9" sewer 950 feet long was completed. An extension of this sewer 1,400 feet long through the Cemeteries Valley was made to give connections at the boundaries of certain lots in the above area.

205. *Hong Kong Travel Association Kiosk*:—The site chosen for this information bureau was already occupied by the depot of the Imports and Exports Department; this was demolished and rebuilt in combination with the Kiosk. The building was two stories high, built with reinforced concrete framing and hollow block floors and walls, finished externally in Shanghai plaster. Accommodation included information bureau and office for the Hong Kong Travel Association and depot and office for the Imports and Exports Department. A contract was let in July to Messrs. Hop Shing & Co. Satisfactory progress was made and the building was completed in October.

Expenditure ..... \$1,900.00

206. *Reconstruction of Kowloon City Ferry Pier*:—A contract for the construction of a reinforced concrete pier 80' x 32' was awarded to Messrs. Lam Woo & Co. on the 21st October. A temporary timber pier to provide facilities for operating the ferry service was firstly built on the adjacent site. By the end of the year, all the piles were driven, walings and braces were concreted and the decking of the permanent pier was formed.

Expenditure ..... \$5,487.60

## — Q 47 —

172. *New Cemetery No. 7*:—The work carried out under this Contract consisted of the formation of terraces, access paths and draining a new section of this cemetery.

Expenditure .....\$14,396.64

173. *Street Name Plates*:—Five additional name plates were erected from stock in hand and a further thirty-five name plates were ordered. The latter were not received in time for erection during the year.

Expenditure .....\$634.53

174. *Kowloon City Police Station—Rewiring*:—The building was wired to serve 255 points.

Expenditure .....\$2,895.42

## WATER WORKS.

175. *Back Lane Service Pipes*:—In addition to two services which were enlarged from 1½" to 2", eight new or extended services were laid. This involved the laying of 1,260 feet of 2" and eighteen feet of 1½" wrought iron piping. Fifty new services and forty-five existing services were connected to these and existing subsidiary mains.

Expenditure .....\$1,453.76

176. *Distribution Mains*:—The following work was carried out:—697 feet of 12" spun iron main were laid in Kiu Kiang Street between Castle Peak Road and Fuk Wah Street; 144 feet of 6" spun iron main were laid in Kiu Kiang Street east of Castle Peak Road, and 315 feet of 6" cast iron main were removed from Kiu Kiang Street west of Castle Peak Road.

Expenditure .....\$4,235.24

## PORT WORKS.

177. *Refuse Dump at Kun Tong*:—This was referred to in paragraph 183 of last year's Report. No constructional work was carried out during the year. A light railway was laid to expedite the handling of the refuse but this method of transporting the material was later abandoned. Approximately 7,000 cubic yards of dredgings were spread over the refuse dump.

Expenditure .....\$ 3,218.61

Expenditure to 31st Dec., 1937 .....\$26,034.94

178. *Kowloon City Pier*:—This work was referred to in paragraph 206 of last year's Report. The pier was completed and opened for passenger traffic in March.

Expenditure .....\$ 7,150.97

Expenditure to 31st Dec., 1937 .....12,638.57

香港、深圳市及東莞市與龍津石橋相似的石橋

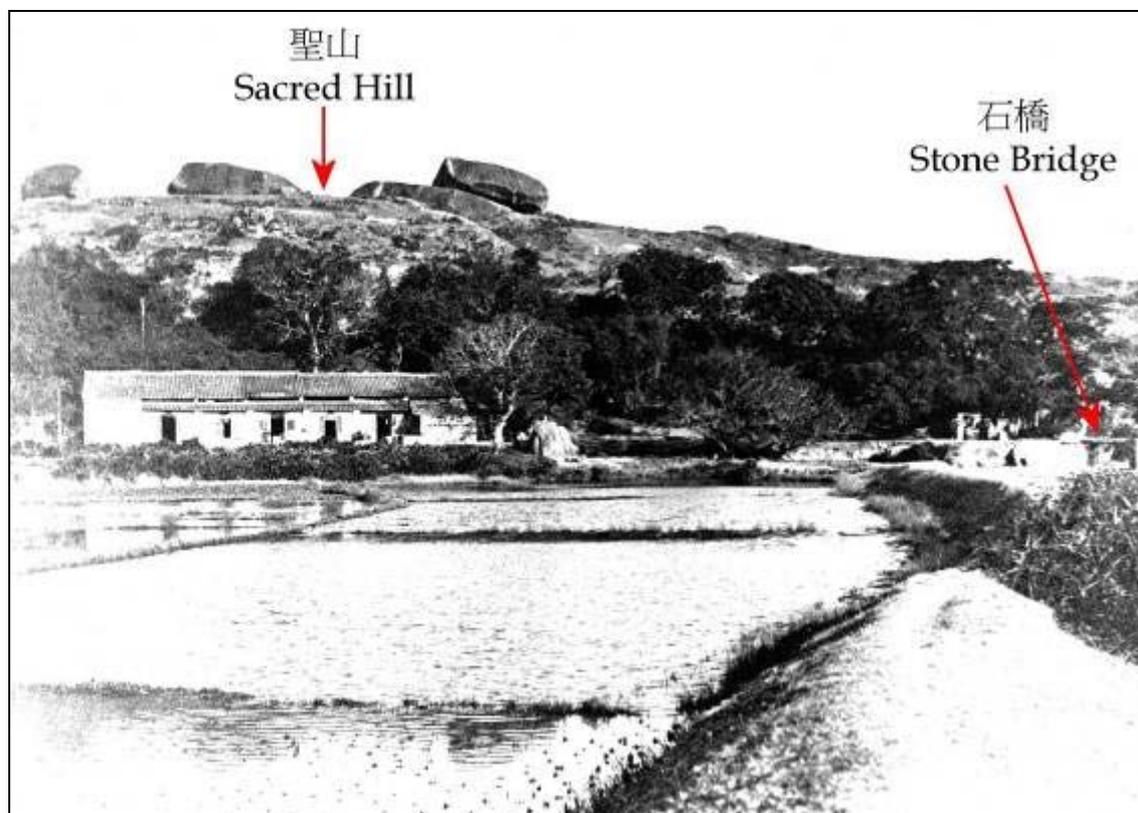
**Stone Bridges similar to Lung Tsun Stone Bridge in Hong Kong,  
Shenzhen City and Dongguan City**

1. 二十世紀初九龍城的面貌，聖山及其周遭  
Kowloon City in early 20<sup>th</sup> Century, Sacred Hill and the surrounding area
2. 二十世紀初的聖山，右方見一石橋  
Sacred Hill in early 20<sup>th</sup> century, a stone bridge on the right
3. 聖山及石橋  
Sacred Hill and a stone bridge
4. 聖山及石橋在 1922-1935 年的測量圖 (CXXXVII-NE-11)  
Sacred Hill and a stone bridge in Survey Sheet CXXXVII-NE-11, 1922-1935
5. 元朗錦田水頭村便母橋  
Bin Mo Bridge, Shui Tau Tsuen, Kam Tin, Yuen Long
6. 元朗錦田水頭村便母橋  
Bin Mo Bridge, Shui Tau Tsuen, Kam Tin, Yuen Long
7. 元朗八鄉水盞田利達橋  
Lee Tat Bridge, Shui Tsan Tin, Pat Heung, Yuen Long
8. 元朗八鄉水盞田利達橋  
Lee Tat Bridge, Shui Tsan Tin, Pat Heung, Yuen Long
9. 沙頭角蓮麻坑古橋  
Old Bridge, Lin Ma Hang, Sha Tau Kok
10. 元朗十八鄉會仙橋  
Wui Sin Bridge, Shap Pat Heung, Yuen Long
11. 元朗十八鄉會仙橋  
Wui Sin Bridge, Shap Pat Heung, Yuen Long
12. 深圳市大鵬所城西登雲橋  
Dengyun Bridge in Dapeng Suocheng Xi, Shenzhen City
13. 深圳市大鵬所城東榮蔭橋  
Rongyin Bridge in Dapeng Suocheng Dong, Shenzhen City
14. 東莞市福慶橋 (中堂·袁家涌)  
Fuqing Bridge (Yuanjiachong Village, Zhongtang Town). Dongguan City



二十世紀初九龍城的面貌，聖山及其周遭

Kowloon City in early 20<sup>th</sup> Century, Sacred Hill and the surrounding area  
(Hong Kong Public Libraries Photo # 752076)



二十世紀初的聖山，右方見一石橋

Sacred Hill in early 20<sup>th</sup> century, a stone bridge on the right  
(Zhou, & Zhong, 2001, p.10)



聖山及石橋

Sacred Hill and a stone bridge  
(Hong Kong Public Libraries Photo # 752077)



聖山及石橋在 1922-1935 年的測量圖 (CXXXVII-NE-11)

Sacred Hill and a stone bridge in Survey Sheet CXXXVII-NE-11, 1922-1935



元朗錦田水頭村便母橋  
**Bin Mo Bridge, Shui Tau Tsuen, Kam Tin, Yuen Long**



元朗錦田水頭村便母橋  
**Bin Mo Bridge, Shui Tau Tsuen, Kam Tin, Yuen Long**



元朗八鄉水盞田利達橋  
**Lee Tat Bridge, Shui Tsan Tin, Pat Heung, Yuen Long**



元朗八鄉水盞田利達橋  
**Lee Tat Bridge, Shui Tsan Tin, Pat Heung, Yuen Long**



沙頭角蓮麻坑古橋  
Old Bridge, Lin Ma Hang, Sha Tau Kok



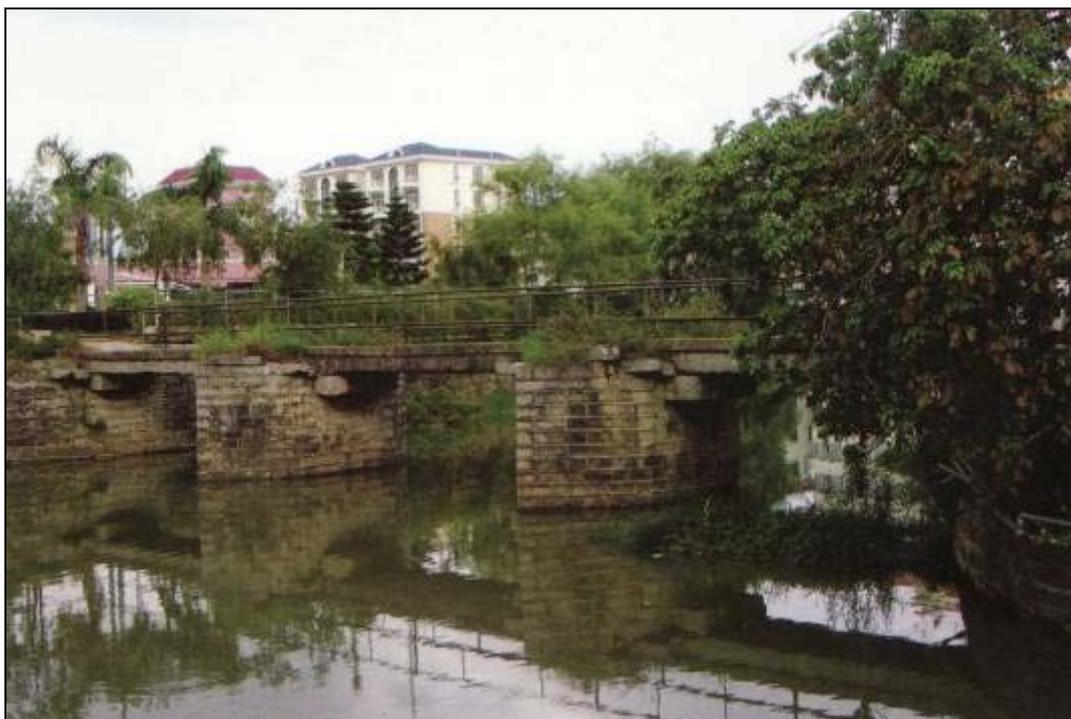
元朗十八鄉會仙橋  
Wui Sin Bridge, Shap Pat Heung, Yuen Long



元朗十八鄉會仙橋  
**Wui Sin Bridge, Shap Pat Heung, Yuen Long**



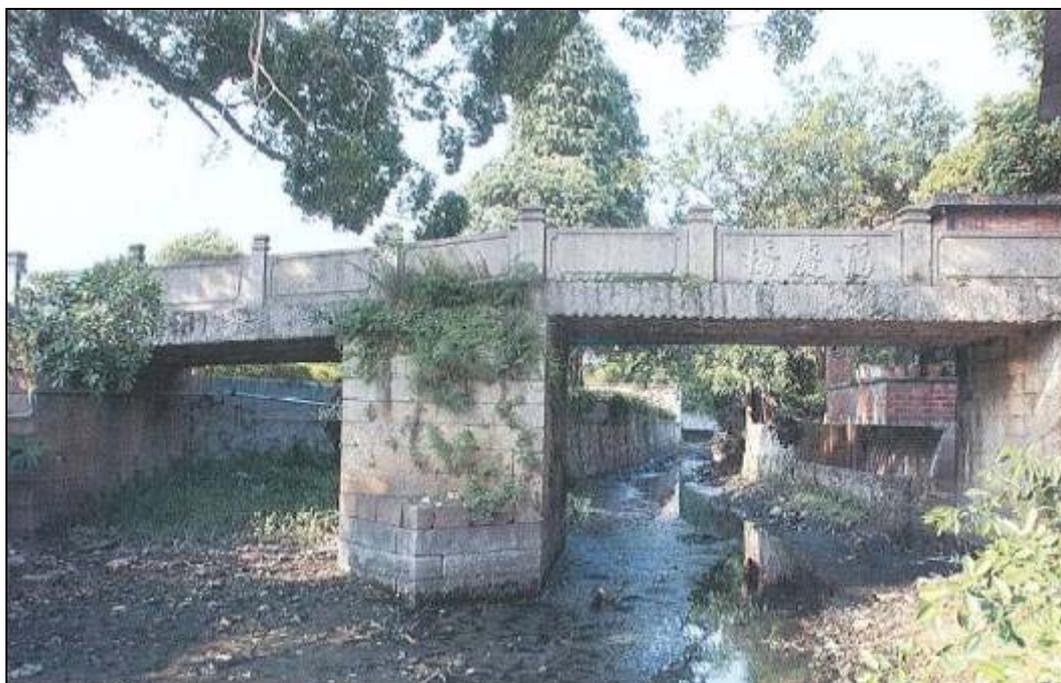
深圳市大鵬所城西登雲橋  
**Dengyun Bridge in Dapeng Suocheng Xi, Shenzhen City**  
(Li, Zhang, & Yang, 2005, p.28)



深圳市大鵬所城東榮蔭橋

**Rongyin Bridge in Dapeng Suocheng Dong, Shenzhen City**

(Li, Zhang, & Yang, 2005, p.28)



東莞市福慶橋 (中堂·袁家涌)

**Fuqing Bridge (Yuanjiachong Village, Zhongtang Town), Dongguan City**  
(Dongguan Municipal Bureau of Culture and Dongguan Municipal Commission for the  
Preservation of Ancient Monuments, 2005, p. 101)